

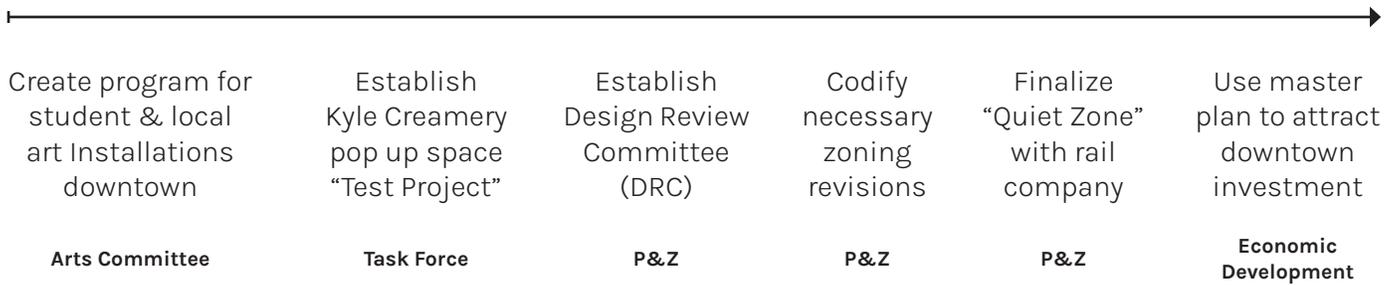
KYLE, TEXAS

Design Standards & Downtown Master Plan

December 2022



Barnes
Gromatzky
Kosarek
Architects



PREFACE

How to use this document:

This document consists of two sections: The City of Kyle Design Standards and The City of Kyle Downtown Master Plan.

The City of Kyle Design Standards should be separated from the Downtown Master Plan and shared with any individual or group intending to significantly renovate or modify an existing building or build new anywhere outside of the downtown area.

The Downtown Master Plan should be shared with any individuals or groups intending to renovate, modify, or build new within the downtown area. For all projects within the downtown area, both the Design Standards and the Downtown Master Plan should be referenced and followed.





ACKNOWLEDGMENTS

The Design Standards and accompanying Downtown Master Plan are a result of invaluable input, expertise, and collaboration between many organizations and individuals. It was made possible with key assistance, guidance, and input from the following:

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EXECUTIVE SUMMARY

EXECUTIVE SUMMARY

Introduction

City Council and Staff have worked with private sector developers and businesses to attract and secure significant investment in the Kyle Community. A large number of residential developments, along with key commercial, retail, and institutional additions have elevated Kyle from small town, to major contributor in the growth and success of Central Texas.

City Council and Staff understand exponential growth must be carefully and strategically guided to support the Community's shared vision for a safe, healthy, prosperous, and self-reliant Kyle. While growth may be inevitable, low quality construction and bad design are not. In 2021, the City hired a team of architects and planners to help create a clear set of design guidelines ("Design Standards") alongside a new vision for Downtown Kyle known as the "Downtown Master Plan". These two complimentary efforts will work in conjunction with the Vybe trail network to make Kyle, Texas as exceptional and unique as its history and charm.

Design Standards

Communities and development projects have been planned or built in nearly every area within Kyle's city limits. New construction has mainly been in areas along I-35, and in undisturbed greenfield sites outside of downtown. In and around the downtown core, there remain a number underdeveloped and underutilized parcels that are in the earliest stages of revitalization and transition. While the original goal of the design standards was to encourage good and appropriate design downtown, it was expanded to include all areas of Kyle.

Four unique areas are identified within the Design Standards, each with its own appropriate set of standards:

- I. Downtown Core / CBD
- II. I-35 Corridor
- III. Trail-Oriented Development & Vybes
- IV. Kyle Suburbs

While each of these areas is individually addressed due to its context and profile, a common thread between the four is that good design does not have to be extravagant. Simple, smart strategies can allow a city to evolve carefully and pay long-term dividends.

The following strategies consider current and future residents, and pave ways to increase the quality of life, while protecting their investments in this thriving community:

The goals of the design standards include:

- Celebrate Kyle's history and charm
- Establish a unique and cohesive Kyle Style including high-quality, honest building materials
- Protect or increase property values
- Attract more shopping and dining options
- Ensure design quality is maintained or improved
- Ensure "Walkability" in neighborhoods through pedestrian safety and comfort

EXECUTIVE SUMMARY

Downtown Master Plan

Kyle, Texas is growing exponentially, but as is common in many cities across Texas and America, attention and investment within the downtown area has not kept pace with development in the surrounding areas and suburbs: it's simpler to develop large greenfield sites saving developers time and money and there is often less public opposition. Conversely, developing an infill project in a downtown area is more expensive, more complex, and because it often involves many neighbors and stakeholders, more scrutinized.

Regardless, development projects in downtown Kyle are inevitable. The city still benefits greatly from its great location, recent growth, and strong leadership. As greenfield sites become more scarce and demand for housing and services in central Texas continues, downtown Kyle is the next logical opportunity for growth. City Staff and Council recognize development is inevitable, and that it should be cohesive, appropriate, and gradual.

This master plan document is a reference tool that establishes strong design and planning principles for the 12-block area defined by city leadership as "Downtown". The goals of the master plan include those of City Council, City Staff and leadership, as well as Kyle's many residents.

The goals of the downtown master plan include:

- Build a Framework for Great Spaces and Places
- Promote greater diversity of land uses
- Align with Kyle's planned Vybe trail network
- Establish Mary Kyle Hartson Park as the heart of Kyle
- Consider short-term and long-term phasing
- Allow residents living downtown currently to remain in their current situations as long as they desire
- Improve pedestrian safety
- Introduce residential options including apartments and townhouses
- Make downtown family-friendly
- Create a unique and comfortable feel that speaks to Kyle's local charm
- Include large sidewalks to support retailers and restaurants
- Consider current and future parking needs



Mary Kyle Hartson, Mayor of Kyle 1937-1947



Downtown Master Plan Project Boundary



DESIGN STANDARDS

December 2022



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1.2 FOCUS AREAS

As Kyle grows, the types of projects that populate it will become increasingly varied. While this potential for variation is exciting, it will be important to maintain cohesion of design in areas of Kyle that are naturally connected. Therefore, four areas that possess this connective quality have been identified in the City of Kyle:

- I. **Downtown Core / CBD**
- II. **I-35 Corridor**
- III. **Trail-Oriented Development (TOD) & Vybes**
- IV. **Kyle Suburbs**

Focus Areas : Summary

Each Focus Area has a certain character that will be enhanced and maintained by the application of its specific standards.

The Downtown Core / CBD centers around Downtown Kyle and is aimed towards helping it become a destination that reflects Kyle's character.

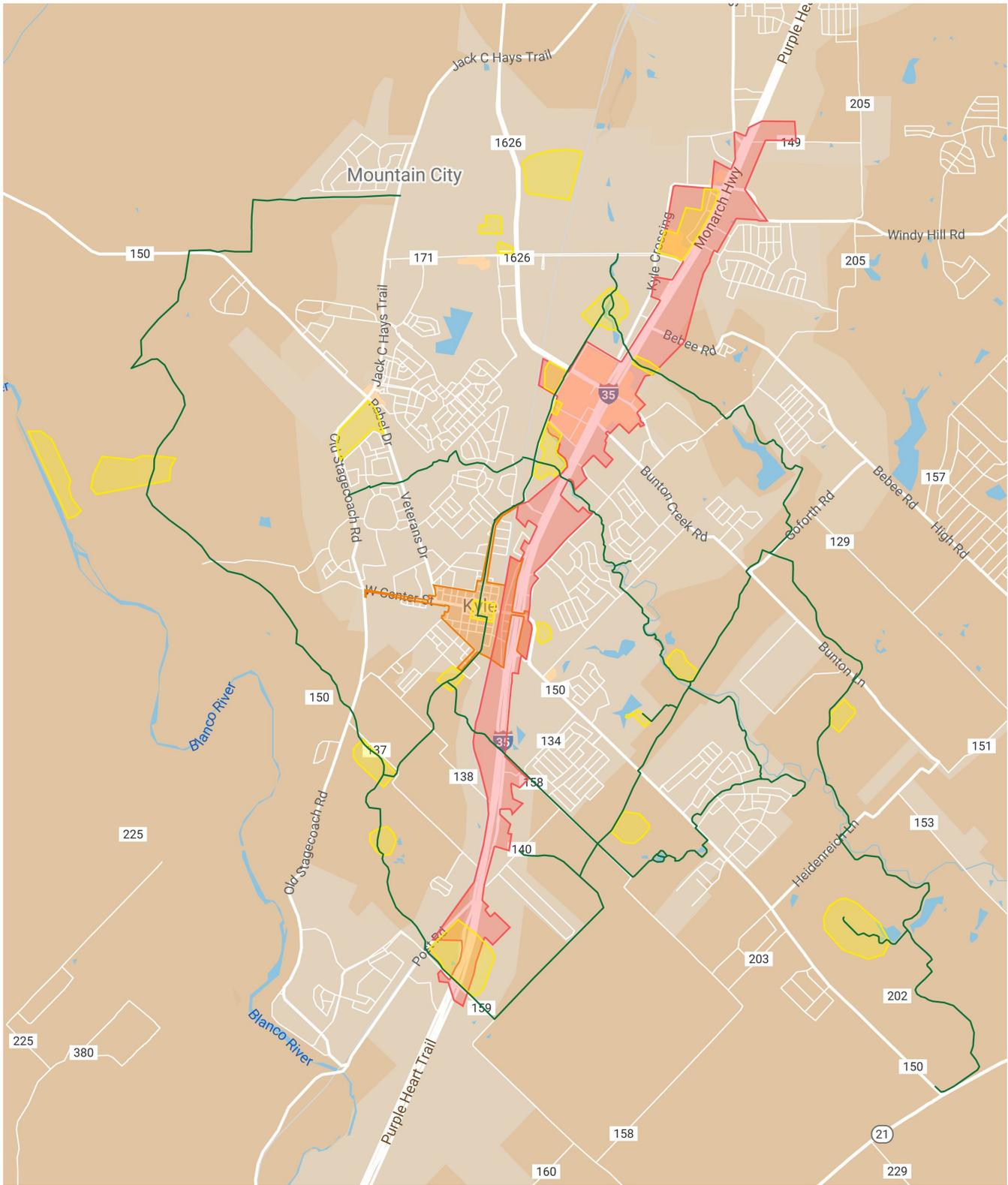
The I-35 Corridor promotes responsible development along I-35 within Kyle's city limits, with the hope of expanding and modernizing the frontage architecture.

The Trail-Oriented Development (TOD) & Vybes will add zoning and new guidelines to ensure environmentally-friendly and sustainable development around trail spaces and Vybes.

The Kyle Suburbs encompasses the rest of Kyle, and will speak to the variety of spaces that can be found throughout it. The standards for the Kyle Suburbs will be less uniform than the other focus areas due to the area's size and diversity of uses, and therefore will provide more flexibility to accommodate the range of projects that will be found there.

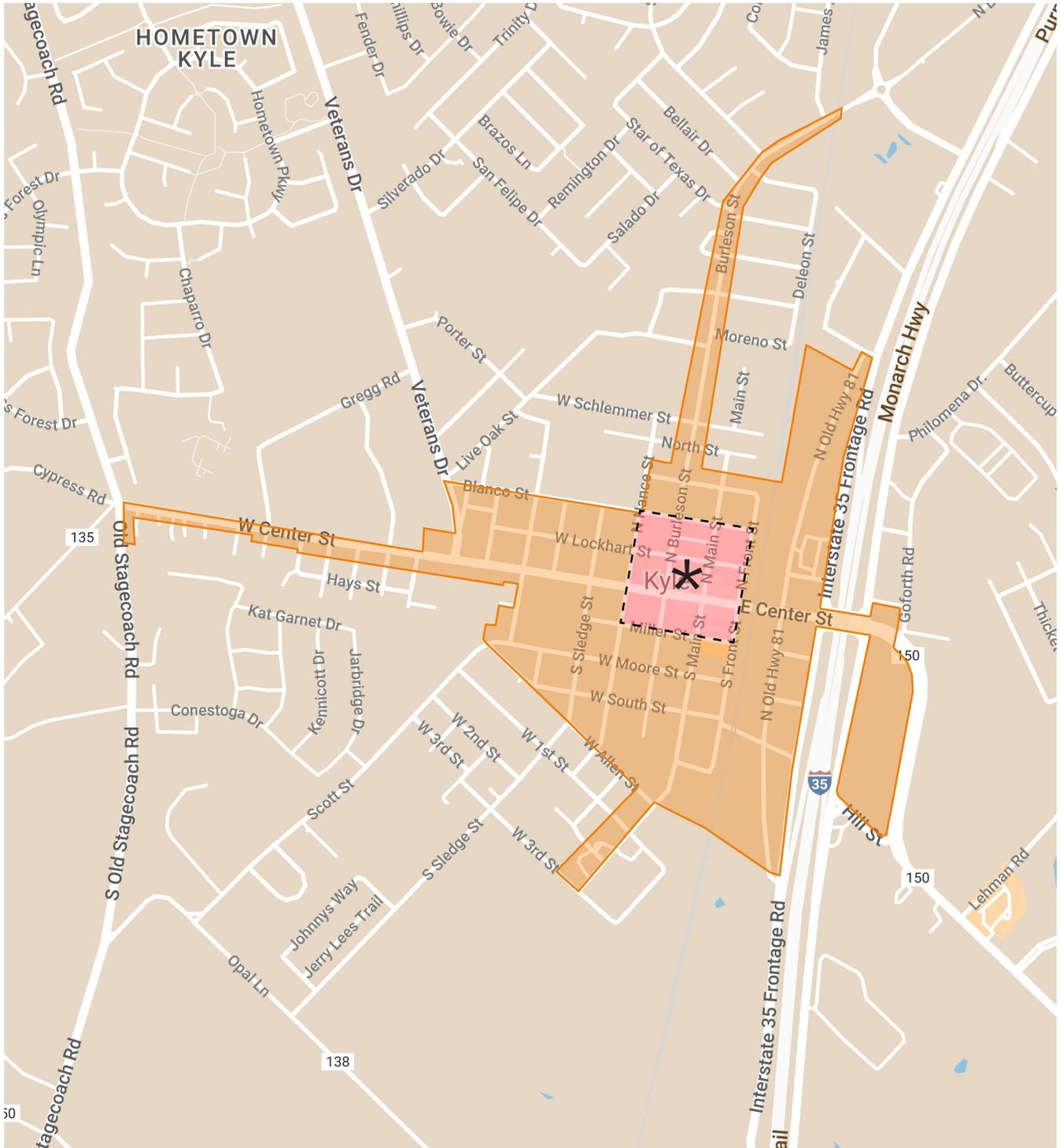
This level of care will ensure any project in Kyle achieves a new standard for quality and durability.

Focus Areas : Mapped



- Downtown Core / CBD
- I-35 Corridor
- Vybe Spots
- Trails

FOCUS 1 : DOWNTOWN CORE / CBD



* Refer to the Downtown Master Plan for requirements in this area

Downtown Core / CBD

Planning for the inevitable growth of Downtown Kyle, these design standards expand the CBD to ensure a standard of design character and quality is maintained. Downtown's charm will be retained but modernized and made attractive to investment.

1. Expand CBD-1 and CBD-2
2. Include area east of I-35
3. Extend CBD north and south to promote improved development projects along the Burleson Vybe Trail
4. Extend CBD to include areas along Center Street currently zoned for manufactured homes (M-2) and Retail/Service (R/S)



Residential Example, ADHOC Architects

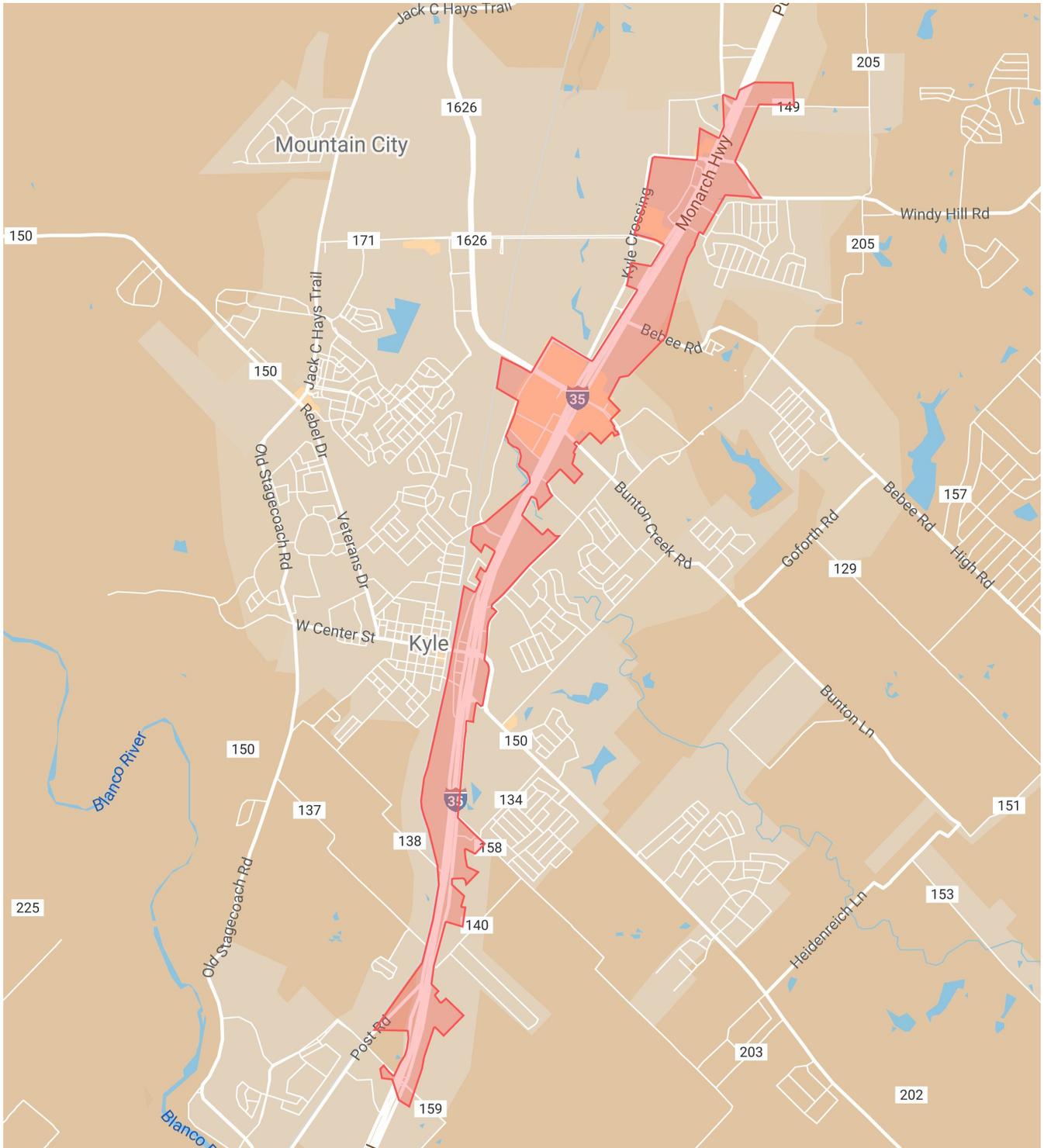


Outdoor Dining Streetscape



Render of the New Mary Kyle Hartson Park, Coleman & Associates

FOCUS 2 : I-35 CORRIDOR



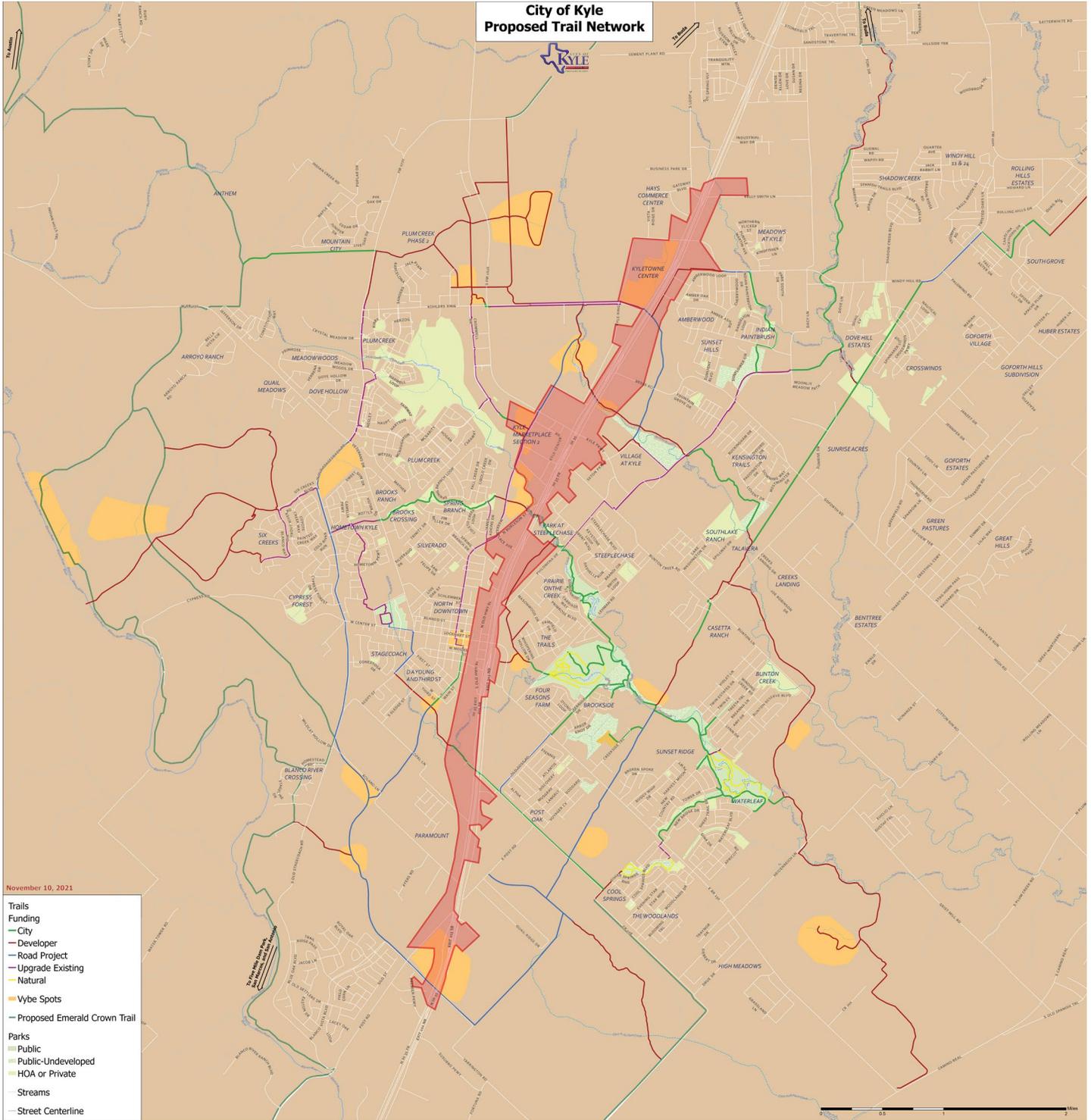
I-35 Corridor

The area along the I-35 corridor is rapidly developing. Steps to encourage well-designed projects are critical, as for many visitors, this is their first impression of Kyle.



Northward View of I-35 in Kyle

FOCUS 3 : TRAIL-ORIENTED DEVELOPMENT (TOD) + VYBES



■ Vyb Spots — Trails

Vybes

The Vybe trail system when completed, may become the city's most beloved and utilized feature. Thematic development clusters along the trails ("Vybes") require appropriate standards to ensure their success.

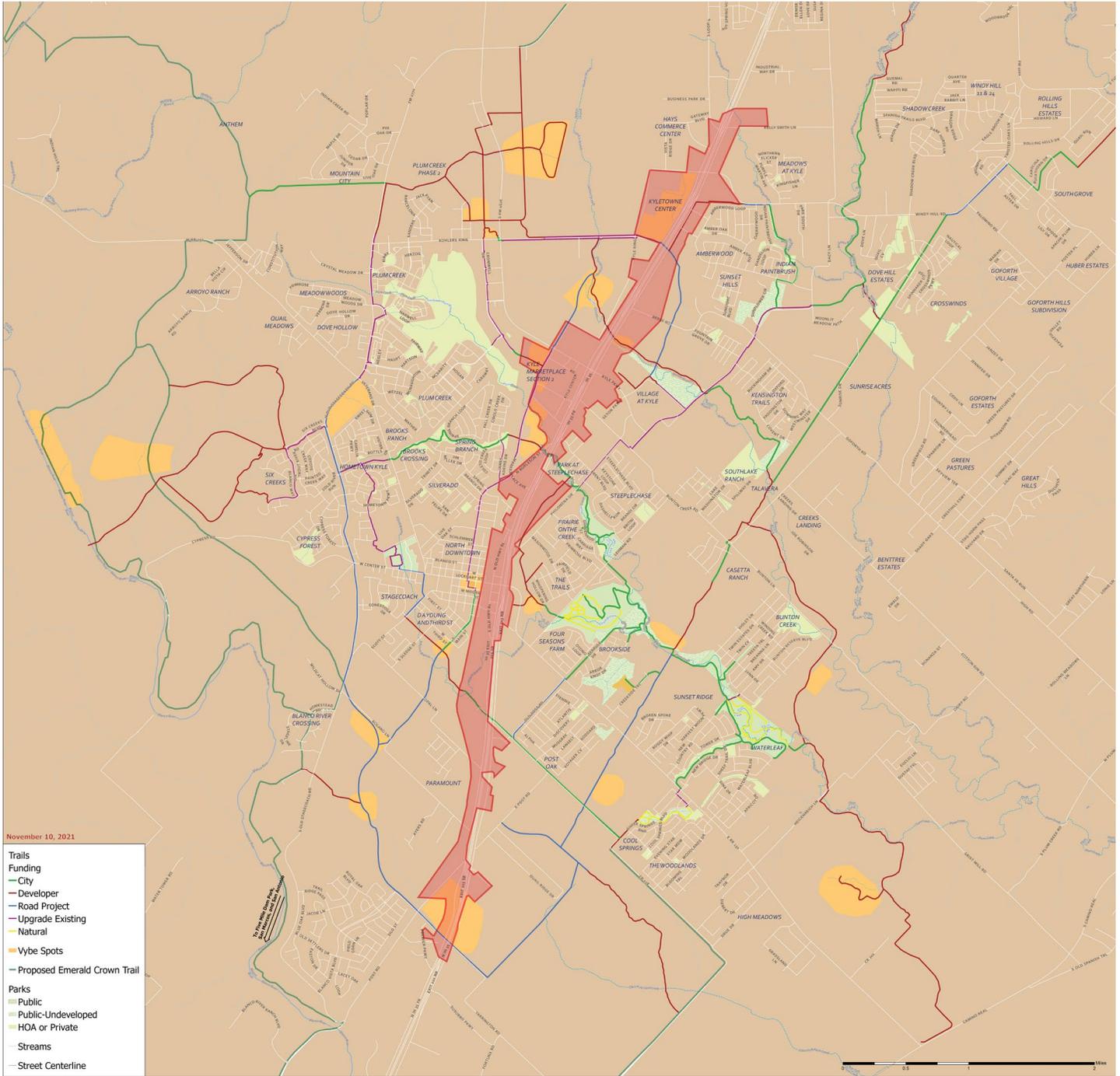


Image from Trail Oriented Development Primer by H-GAC



Press Café in Fort Worth, Texas by EDG Design

FOCUS 4 : KYLE SUBURBS



Suburbs

By acreage, the suburbs are the city's largest opportunity for good design and elevating the quality of life for residents.



Single Family Home by BGK Architects



Agricultural and undeveloped land in Kyle

1.4 BUILDING SITING AND SETBACKS

For guidance on building siting, setbacks, and heights, refer to the division matching the project’s zoning district under Chapter 53, Article II of the Code of Ordinances and these Design Standards.

For example, if a Multifamily project is slated for construction within a Multifamily Residential 2 zoning district, then Chart 1 under Chapter 53, Article II, Division 1 of the Code of Ordinances specifies the project needs:

- Front Setback of 25 feet min.
- Side Setback of 20 feet min.
- Building Height Limit of 45 feet; no more than 3 stories
- Min. Lot Square Footage Area of 12,000 square feet, plus an additional 1,500 square feet for each dwelling unit in excess of four units; provided that the density limitations set forth for each such district shall apply.*

*This note can be found below Chart 1 in the Code of Ordinances; it references the shortened marker (5) shown in the chart.

Chart 1								
Land Use District	Front Setback (feet)	Side Setback (feet)	Corner Lot at Side Street or Alleyway Setback (feet)	Street Side Yard Setback (feet)	Rear Setback (feet)	Min. Lot Square Footage Area	Min. Lot Street Line Width (feet)	Height Limit (feet)
A	40	25	40	25	25	43,500	150	45
UE	25	25	25	25	25	22,500	100	45
R-1-1	35 ⁹	10	20	15	20	8,190 ¹	80 ¹	35
R-1-2	25 ⁹	7.5	15	10	15	6,825 ¹	65 ¹	35
R-1-3	20 ⁹	5	10	5	10	5,540 ¹	50 ¹	35
R-1-A	25	(2)	10	15	15	4,550 ¹	35	35
R-1-T	(3)	(3)	15	15	(3)	880 ³	22	35
R-1-C	(4)	(4)		15	(4)	9,000 ⁴	80	45
R-2	25 ⁹	7	10	15	25	9,000	80	35
R-3-1	25	15	15	15	25	(5)	80	35 ⁶
R-3-2	25	20	15	15	25	(5)	80	45 ⁷
R-3-3	25	7	15	15	25	(13)	90	45 ⁷

1.5 PREFERRED USES

Preferred Uses

Preferred uses - diverse uses - include support retail and services to support needs while enabling walkability for Kyle residents interested in the option. More residential and commercial spaces within a given area, allows for people to walk from place to place for health benefits, decreasing their carbon footprint and reducing the number of cars in local traffic. Increased pedestrian activity generates social interactions which strengthens communities and increases the chance for local retailers and restaurants to succeed. The greater the variety of uses in a given area, the more likely neighboring businesses will benefit, resulting in greater sales and tax revenue.

Downtown Core

Residential: (Specify for existing homes) Single-Family Residential 1, (Specify for existing homes) Single-Family Residential 2, (Specify for existing homes) Single-Family Attached, Residential Townhouse, Residential Condominium, Residential Two-Family, Multifamily Residential 1, Multifamily Residential 2, Apartments Residential 3

Commercial/Other: Central business district 1, Central business district 2, Entertainment, Mixed-Use, Office/Institutional, Retail and Services. Prohibit strip centers, outdoor storage, self storage, fast food, drive thrus, big box retail.

Expand to include hotel and suggested uses in Downtown Master Plan

Special incentives made available for retail and service providers with a focus on health.

I-35 East

Area across from downtown is considered an extension of downtown - a special district, included in the CBD.

Preference for clustered mixed-use development including retail, apartments, office, and ample outdoor space.

Prohibit strip centers, outdoor storage, self storage, fast food, and big box retail.

I-35 Corridor

Residential: Residential Condominium, Multifamily Residential 2, Apartments Residential 3

Commercial/Other: Billboard, Community Commercial District, Entertainment, Mixed-Use, Neighborhood Commercial District, Office/Institutional, Retail and Services

Trail-Oriented Developments (TODs) and Vybes

Residential: Residential Condominium, Multifamily Residential 1, Residential Two-Family

Commercial/Other: Community Commercial District, Mixed-Use, Neighborhood Commercial District, Retail and Services

Kyle Suburbs

Residential: All residential zoning uses permitted

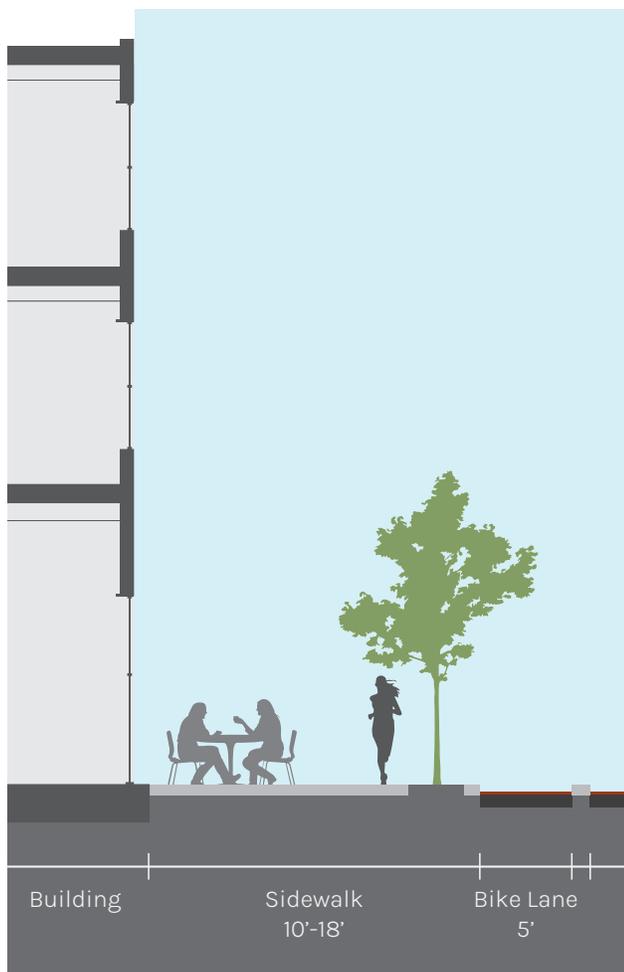
Commercial/Other: All other zoning uses permitted

1.6 PARKING DESIGN

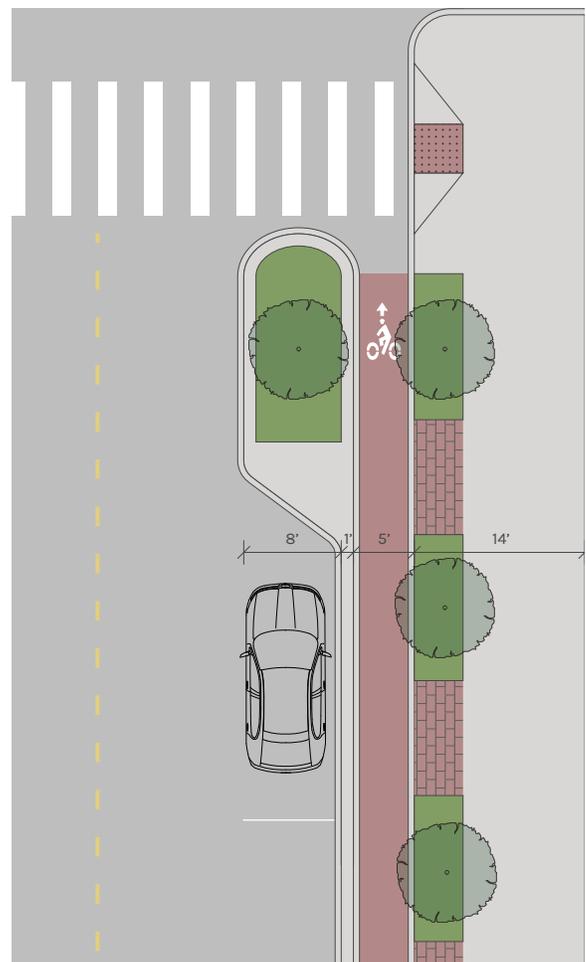
Pedestrian-Parking Balance

For all focus areas, a greater balance between automobiles, pedestrians, bicyclists, golf carts, and other modes of transportation is necessary.

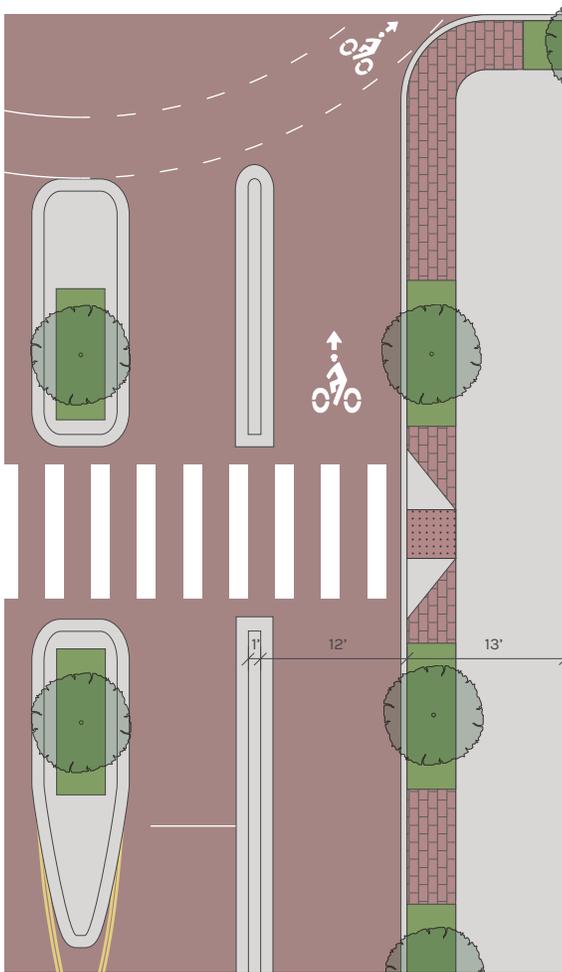
Strategies to ensure public safety and relieve traffic congestion include the Vybe Trail, new bike lanes, and “on-street” parking locations including diagonal and parallel spaces.



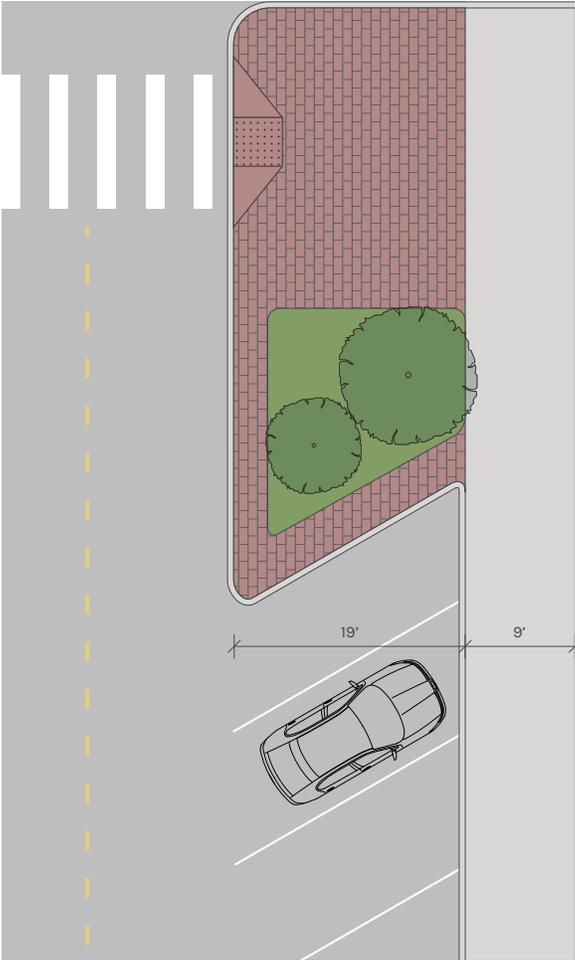
Building-Sidewalk-Bike Lane Condition



Parallel Parking and Bike Lane Condition



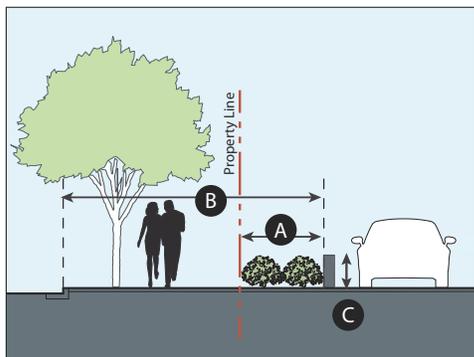
Burselon Street Condition



Angled Street Parking Condition

Parking Lots Along Streets

Surface parking areas adjacent to public streets should include a buffer of 15'-0" minimum from face of curb including a 5'-0" landscape / planting zone, and a 30" screen wall for permanent surface lots.



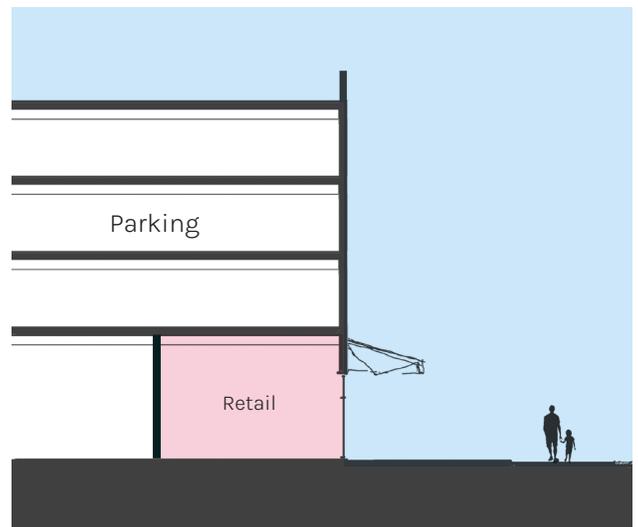
- A** Minimum setback of 5'-0 with landscapes
- B** Minimum 15'-0" from face of curb
- C** 30" Screen Wall for permanent surface lots

Example Diagram

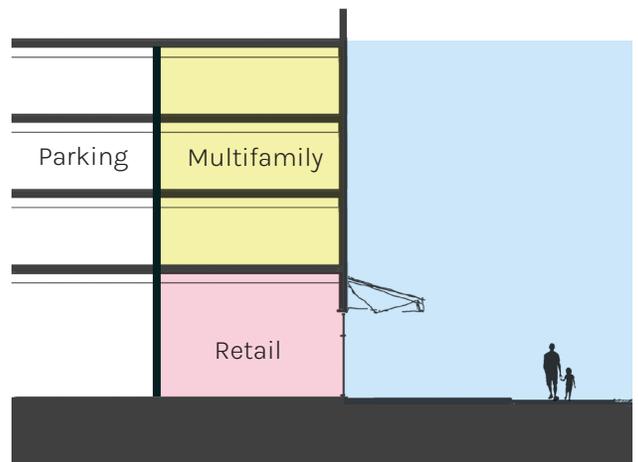
Parking Garages on Pedestrian Corridors

Parking garages should include pedestrian intensive design at the first floor level. Long elevations should include some combination of retail tenant space, spandrel glass, awnings, or an attached grid system for growing climbing plants vertically to screen the building.

Car headlights should be screened on all levels to prevent light from leaving the building and disrupting adjacent neighbors.



Parking Garage Pedestrian Focus



Wrap Building Pedestrian Focus

Parking Lots : Focus Areas

I-35 Corridor

- Parking Lots with 20 or more proposed parking space shall have a minimum percentage of 15 percent of the total lot area devoted to landscape development. Preservation of existing trees and vegetation will be given priority.
- Parking lots with 20 or more proposed parking spaces will have a minimum of 1 tree per 5 parking lot spaces.
- Parking lots with less than 20 parking spaces are required to have a minimum of one tree per 7 parking spaces.
- A minimum of 65 percent of the required parking lot trees will be located within the interior of the surface level parking lot.
- The minimum size allowed for a landscaped parking lot island is 9' x 19' to allow for healthy growth by tree(s) and plantings located there. Parking lot islands can be reduced to 9' x 17' if necessary.
- Parking lot islands are required at a maximum of every 60 feet.
- Continuous, landscaped islands are required between every other interior parking bay. Trees should be planted in these islands every 25 feet.
- To support viability of parking lot shade trees, a minimum 24" soil depth and 250 cubic feet of soil is recommended per tree, with topsoil mounded to a center height with a 1 to 3 percent slope above the top of the curb.
- Bicycle storage space must be provided in conjunction with parking lot. There should be at least 4 bike storage spaces per project site or 2 bike storage spaces per business, whichever is greater.
- Impervious paving materials prohibited for greater than 10 percent of surface parking lot. Material selection for remaining area surface parking lot should consist of porous pavement or alternative pavers.



Calvados-Honfleur Business Park, La Compagnie du Paysage



Well-Shaded Parking Lot, Chattanooga, TN

Trail-Oriented Developments and Vybes

- Bicycle storage space must be provided in conjunction with parking lot. There should be at least 4 bike storage spaces per project site or 2 bike storage spaces per business, whichever is greater.
- Impervious paving materials prohibited for greater than 5 percent of lot. Material selection for remaining driveable surface of parking lot should consist of porous pavement or alternative pavers. For TODs and Vybes, alternative pavers are preferred.
- Porous Pavements options: pervious concrete (recommended for Central Texas conditions) and porous asphalt
- Alternative Paver options: TRUEGRID™, Turfstone™, Checker Block™, Gravelpave², and Grasspave².
- If using an alternative paver system, fine gravel, decomposed granite, or another local, rocky substance should be used in place of grass due to maintenance issues and environmental tolerance. If grass infill is used, it must be in an area that is shaded at least 50% of the time to ensure grass receives adequate protection from sun.
- Parking spots for disabled individuals must be constructed with porous pavement or impervious paving (counts towards 5 percent maximum) for ease of use and maintenance of accessibility.
- Stormwater Management should rely primarily on the use of: swales, vegetated filter strips, bioretention areas (rain gardens), dry retention basins, and/or wet retention basins. A combination of strategies can be acceptable.
- Parking lots smaller than 30 cars to rely on swales with a perimeter of vegetated filter strips and/or rain gardens, while larger parking lots should use these in conjunction with a dry retention basin.



Green Parking for Lidl de Longpont-Sur-Orge, ECOVEGETAL



Woodstock Downtown Plaza, TSW Design



Parking area utilizing permeable paving system

1.7 BUILDING CHARACTER AND FACADES

Building Design Principles

- The identity of individual buildings should be reinforced with expressive architectural detail.
- Buildings should be sited and designed to provide strengthened definition to the downtown's framework of streets and open spaces.
- The massing of the buildings should vary in form and volume to bolster the scale of an urban downtown.
- Buildings should be constructed with a level of care and craft that utilizes durable, sustainable materials.
- While buildings that utilize traditional form and materials is acceptable, it is also encouraged to consider modern interpretations of Central Texas vernacular architecture.
- Buildings with multiple levels should have definition between its base, middle and upper level(s).
- Roof height, parapet height, materials, window composition, and roof treatments should be used to provide definition to building volumes.
- Maintain regional authenticity, and design appropriately for the context (place and time).
- Strive for the inclusion of local character. Buildings should be designed to respond to the local climate.
- Use high quality materials and promote quality construction in order to make structures as maintenance-free as possible.
- Buildings should provide opportunities for multi-tenant, pedestrian-oriented development at the street level.
- Urban buildings should be built out to their property lines when possible.
- Drive-thru facilities are not permitted in the Downtown Core.
- Encourage local artist and artisan participation in detailing and materials.
- Integrate flexibility into the building design to allow for future alterations.

Roof Treatment

Buildings make use of flat or sloping roofs, however, sloping roofs are encouraged/ permitted on buildings three floors or less. Sloped roofs may be constructed with standing seam metal roofs, concrete/ clay tile, or other complimentary materials deemed by City Architect. Sloped roofs shall be designed with generous overhangs, exposed beams, decorative brackets of materials which are complimentary.

Example Building Forms



Residential / Mixed-Use, ADHOC Architects



Multifamily "Wrap Building", Mithun



Boutique, Furman + Keil Architects



Civic Building, BGK Architects



Unique Vybes, TSC Architects



I-35 Corridor, LRS Architects

Facade Design

- Building facades are to be articulated and composed such that there is a distinction between the pedestrian scale and the building scale. Create visual continuity with adjacent and neighboring buildings, and engage the pedestrian streetscape.
- To reduce light pollution, uplighting of facade and exterior elements are not permitted.
- No painted brick. Building exteriors should be neutral or natural tones unless approval is sought and granted.
- First floors should generally be taller than latter floors. Lower floors should be distinct from the upper floors from the building's exterior.



Example Facade, Wang Architects

Retail Signage

A comprehensive coordinated signage program shall be prepared and submitted to the City Architect/P&Z for their review and approval.

Design Standards to include:

- Simplified allowable types, colors, sizes of retail signage based on location
- Establish a hierarchy of signage regulations, with the most stringent requirements for signage around Mary Kyle Hartson Park. More flexibility granted at CBD, Kyle Suburbs, and most flexibility at the I-35 Corridor Area:
- See appendix



Example Signage

1.8 MIXED-USE BUILDINGS

As the City of Kyle grows, so will the opportunity for collaborative spaces in the city. Mixed-Use architecture will afford residents the ability to live, work, and play in high-quality and walkable communities.

There are five categories of spaces that will be appropriate in a mixed-use structure within the City of Kyle: Household Living (Non-commercial), Institutional Living (Commercial), Civic and Institutional, Commercial, and Other (such as Wireless Communication Facilities). For a list of the allowable uses within each category, as well as the code applicable to mixed-use buildings in Kyle, please refer to Chapter 53, Article II, Division 26 of the Kyle, Texas Code of Ordinances.

The following sections will outline expectations for the use of these categories in mixed-use structures, as well as provide detailed guidelines for their design in each focus area.

Design Objectives

- Provide residential occupants with security, privacy, and a friendly neighborhood to foster a sense of home.
- Create safe, engaging pedestrian spaces for the community to enjoy.
- Ensure compatibility in aesthetic and building occupancy with neighboring land-uses.

Requirements for all Mixed-Use Buildings

These requirements apply to all new mixed-use construction in the City of Kyle, regardless of the focus area the building is located in:

- The first floor height to the finished ceiling must be at least 11 feet to create a distinction for these spaces from the upper floors.
- Rooftops can be used as accessible outdoor space for residential and commercial developments. At least 30 percent of occupiable square footage must be adequately shaded. Additionally, at least 10 percent of square footage (occupiable or not) must have landscaping (zeroscaping is an allowable form of landscaping).
- Loading/service areas and waste management must be concealed from view and located to the rear of the building, accessible through an alleyway or tertiary street to minimize their impact on the pedestrian experience. Waste management areas must be screened with acceptable materials (found in materials section).



Bill Sorro Community Building, Kennerly Architecture and Planning

Mixed-Use : Focus Areas

Downtown Core/CBD

- A combination of the Household Living, Institutional Living, and Commercial categories for mixed-use structures are permitted within this area. Ground floor spaces are limited to allowable commercial uses, dining, and public spaces such as galleries. Subsequent floors must be all living spaces or all commercial, specifically office spaces.
- Building form will be limited to 3 stories with a maximum building height of 45 feet with rooftop patio/amenities allowed.
- Ground floor facade must be visually distinct from the second and third floor. The second and third floor facades should be visually similar or identical in order to delineate building usage from the outside.
- Buildings should be constructed as close to the property line as possible, with a “zero setback line” policy encouraged. Buildings may be setback a maximum of 20 feet from the property line for the purposes of: greater pedestrian circulation; outdoor dining and/or seating; or well-maintained landscaping to improve the quality of space for first-floor businesses.
- At least 30 percent of the linear frontage on the main face(s) of the mixed-use building shall be designed for community-serving commercial uses. The minimum interior depth of these spaces shall be 25 feet.
- Entries to first-floor spaces shall be easily accessible from the sidewalk and primary street the building is located on. Entrances to upper floors shall be located on secondary street(s) in interior courtyards and/or well-maintained and visually appealing elevator lobbies and staircases. An entrance to upper floors is permitted along the primary street face as long as the entrance is visually-appealing, discrete, and secure.
- Buildings on corner lots shall have a primary entry facing the intersection.



The Tieton, Lenity Architecture



(Proposed) The Edes Building, KTG Architecture + Planning

I-35 Corridor

- A combination of the Household Living, Institutional Living, Civic and Institutional, and Commercial categories for mixed-use structures are permitted within this area.
- Ground floor spaces are limited to Commercial and/or Civic and Institutional uses, with limited ground-floor uses if upper floors are Institutional Living spaces.
- The second floor may be Commercial, Civic and Institutional, Institutional Living, or Household Living, but the use category must be the same across the entire second floor (combination of categories not permitted). All subsequent floors must be one category: Household Living, Institutional Living, Civic and Institutional, or Commercial.
- Building form will be limited to 5 stories with a maximum building height of 55 feet with rooftop patio/amenities allowed. A maximum building height in excess of 55 feet and exceeding five stories is possible with specific approval via conditional use permit.
- Commercial-floor facades must be visually distinct from subsequent floors. Upper floors should be visually similar or identical to delineate building usage from the outside.
- Buildings should be constructed with at least a 15 foot setback from the property line to distance structure from interstate. Sidewalks must be at least 10 feet wide.
- Entries to commercial spaces shall be easily accessible from the sidewalk located on the primary street. The primary street for the structure must be on the opposite side of the building from that facing the interstate. A maximum of two entrances to upper floors shall be located on the primary street through interior courtyards and/or visually-appealing elevator lobbies and staircases.
- Entries to commercial spaces and upper floors are permitted along face of structure facing interstate, but they should not be the main entries to these spaces and must remain visually-appealing.
- At least 30 percent of the linear frontage facing the primary street shall be designed for community-serving commercial uses. The minimum interior depth of these spaces shall be 25 feet.



MacArthur Heights Modern Mixed-Use, Architects Orange



First Point Apartments, Architects Orange

Trail-Oriented Developments (TODs) and Vybes

- A combination of the Household Living, Institutional Living, Civic and Institutional, and Commercial categories for mixed-use structures are permitted within this area.
- Ground floor spaces are limited to Commercial and/or Civic and Institutional uses, with limited ground-floor uses if upper floors are Institutional Living spaces.
- Upper floors must be all Household Living or Institutional Living (Civic and Institutional and Commercial uses are not permitted).
- Building form will be limited to three stories with a maximum building height of 40 feet with rooftop patio/amenities encouraged.
- Buildings can be constructed between 0 to 25 feet from the property line. Resulting spaces should be designed for the purpose of: greater pedestrian circulation; outdoor dining and/or seating; or well-maintained landscaping to improve the quality of space for first-floor businesses.
- Sidewalk in front of structure must be at least 10 feet wide. Bike racks with a capacity of 15 bikes must be available near the front of the building. At most three parking spots specifically for golf carts/similar recreational vehicles can be provided on premises.
- At least 30 percent of the linear frontage on the main face(s) of the mixed-use building shall be designed for community-serving commercial uses. The minimum interior depth of these spaces shall be 25 feet.
- Entries to first-floor spaces shall be easily accessible from the sidewalk and primary street the building is located on. One entry to upper floors is permitted on primary street of building. Remaining entrances to upper floors shall be located on secondary street(s) in interior courtyards and/or well-maintained and visually appealing elevator lobbies and staircases.



1211 E. 11th Street, Furman + Keil Architects



The Klotski Building, Graham Baba Architects

1.9 RESTORATION & ADAPTIVE REUSE

With the city experiencing so much growth, the question of how existing buildings should be approached becomes extremely relevant. With as close-knit as the community and history of Kyle is, it is vital that each structure be handled in a way that reflects that values of the community. For this reason, demolition should be avoided whenever possible. Instead, the preservation, restoration and adaptive reuse of existing structures is extremely encouraged.

Regarding demolition of any structures, refer to the City of Kyle Code of Ordinances, Sec. 53.839 for guidelines specifying when demolition is appropriate, and the process for receiving a Certificate of Appropriateness required for Demolition.

Restoration, “...focuses on retention of materials from a particular time of the building’s history while removing materials...” and additions that are not architecturally or historically significant (Hernandez).

Adaptive Reuse, on the other hand, “...focuses on retention and repair of historic buildings, but also acknowledges the need to alter and add to a historic building due to deterioration” (Hernandez).

Restoration of a structure should be considered if the building’s architecture is noteworthy, or if it is has a significant history and restoration of the structure would contribute to the preservation of that history. Additionally, restoration should be considered if members of the community deem it significant to their community or history.

Adaptive Reuse of a structure should be considered when it is appropriate to repurpose and/or retrofit the building to adjust to cultural

or economic changes in the surrounding context. Adaptive reuse also tends to be more sustainable because it makes use of the existing building foundation and several materials/built portions of the existing structure. For this reason, if an existing building on a site being developed has the potential to be adaptively reused, it is highly encouraged to do so.

Restoration

- Wherever possible, preserve original building materials and architectural details.
- Existing building materials and architecture that can meet code and remain visually-appealing should be repaired rather than replaced.
- If visually-appealing portions of the structure have been covered, such as a detailed facade that was later covered by plywood and drywall, the safe removal of the covering materials should be prioritized to expose the underlying facade. If the existing facade has suffered as a result of the covering, repairs should be made to restore it to its original character or a very similar appearance.
- If building materials and/or architectural details are in extensive disrepair, than they should be replaced with a similar item that maintains the building’s visual character.
- Any alterations within the restoration process that would imply the building was built earlier than it actually was is prohibited. Restoration should retain the building’s original character and marks of the time period it was built in.

Restoration Project Examples



Before: Small Building on East Street, Built in 1893



After: Bottling Dept. Food Hall, Clayton Korte

“...we were immediately drawn to the exposed brick walls, the beadboard pine ceiling, the expanse of the space, and the possibilities it held....We pulled up an area of the carpet, and, sure enough, there sat the original hardwoods, carefully preserved and making it clear that this was the space for us. Because these existing architectural features are what made us fall in love with the space to begin with, we will be preserving them as much as possible. The building was essentially in ‘shell’ form, so it was easy to maintain this main, open area and remodel the rear build-out area in a way that meets [the space’s needs]” (Williams).



Bottling Dept. Food Hall, Clayton Korte

Recommended Project for Restoration



Before: Abandoned Milk House



After: The Kyle Creamery Pop-Up

Adaptive Reuse

- When considering new uses for an existing structure, it is important to research social and economic factors to discern which uses would support the project's success. While it's vital to consider what programming the existing structure lends itself to, it is also relevant to ensure that the chosen program(s) are fulfilling a social, economic, and/or cultural need in the community so it is maintained after it is built.
- Focus on maintaining the shell and structure of the building, as well as original building materials and architectural details wherever possible. If original materials and details cannot be preserved in their original place in the st, salvage these components and integrate them into the building elsewhere.
- Consult with structural engineers and relevant building consultants to verify structural integrity of building and its parts - if feasible, build upon existing skeleton.
- Existing building materials and architectural details that can meet code and remain visually-appealing should be repaired rather than replaced.
- If visually-appealing portions of the structure have been covered, such as a detailed facade that was later covered by plywood and drywall, the safe removal of the covering materials should be prioritized to expose the underlying facade. If the existing facade has suffered as a result of the covering, repairs should be made to restore it to its original character or a very similar appearance.



Before: Space being used as Small Parking Garage, built in 1920's



After: Inheritance Juicery, Selser Schaefer Architects



Interior of Inheritance Juicery, Selser Schaefer Architects

Adaptive Reuse Project Examples



Interior of Inheritance Juicery, Selser Schaefer Architects



Before: Space being used as Small Parking Garage, built in 1920's

“This 100-year-old building spent the last 30 years as an indoor parking garage before being transformed into the bright, vibrant gathering space it is today. Capitalizing on the building’s existing elements with a focus on natural light, transparency, and community, the story of this building has been re-written for the next 100 years” (Selser Schaefer Architects). The renovation made sure to keep as many original elements from the building as possible. This meant preserving the arched trusses and vaulted ceiling and exposing the existing brick with a balance of plaster touch-ups.



After: Inheritance Juicery, Selser Schaefer Architects

Recommended Project for Adaptive Reuse



Before: Former Kyle Police Headquarters



After: Downtown Multi-Use for the Public

1.10 BUILDING MATERIALS

The purpose of this section is to ensure a high degree of quality, durability, and aesthetic appeal to promote compatible and cohesive design within Kyle and maintain the history and spirit of materials and styles commonly found in Central Texas.

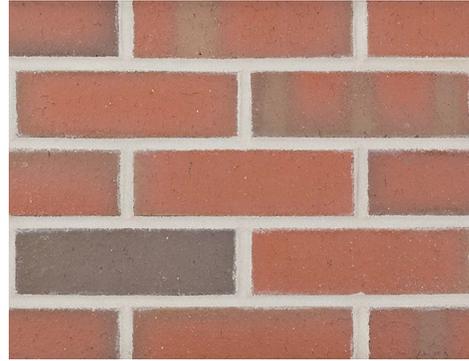
Approved exterior materials include: limestone, brick, smooth faced stucco, light colored precast concrete, metal panel, hot-rolled steel, wood, mass timber and similar high-quality materials. Alternates can be reviewed by the City Architect/P&Z and deemed to be compatible/complimentary.

Exterior materials not permitted:

- Painted masonry of any kind
- Exterior Insulation Finish System (EIFS)
- Plastic or Vinyl Siding
- Plywood
- Oriented Strand Board (OSB)
- Mirrored Glass



Texas Limestone



Brick



Concrete



Smooth Stucco



Hot-Rolled Steel



Mass Timber



Metal Panel



Wood

Material Palette

Primary Brick Blends (standard, mid-century or Queen sizing only)

"Kyle Commons" by Old Texas Brick



The Kyle Commons blend is similar to other buff colored brick blends used throughout central Texas but has been modified to be unique through its inclusion of an occasional red brick as a gesture to Kyle's historic pavers and red brick buildings. This blend, created by BGK with Old Texas Brick, is historical, innovative, and uniquely Kyle.

- *For buildings around MKH Park*
- *To be paired with dark awnings*
- *Specify beige or tan mortar*

"Kyle Civic Blend" by Acme Brick



The Kyle Civic Blend is an evolution of the red brick found in Kyle's paving and more recent civic buildings. The blend includes a variety of deep red hues, to signify new layers of ideas, people, and development, built upon a strong historic foundation.

- *For municipal buildings*
- *To be paired with Texas Limestone*
- *Specify vertical scratch finish*

Other Acceptable Blends (Old Texas Brick in standard, mid-century or Queen sizing only)



Golds Blend



Buff Gold



Old Chicago



Old San Luis



New Orleans



Grey Antique



Espresso Antique



Vintage White



Kyle Commons Facade



Kyle Civic Blend Facade

Limestone Palette



Texas Lueders



Marseille



Mezzo Taupe

Limestone Texture



Split Face



Aged



Chateau

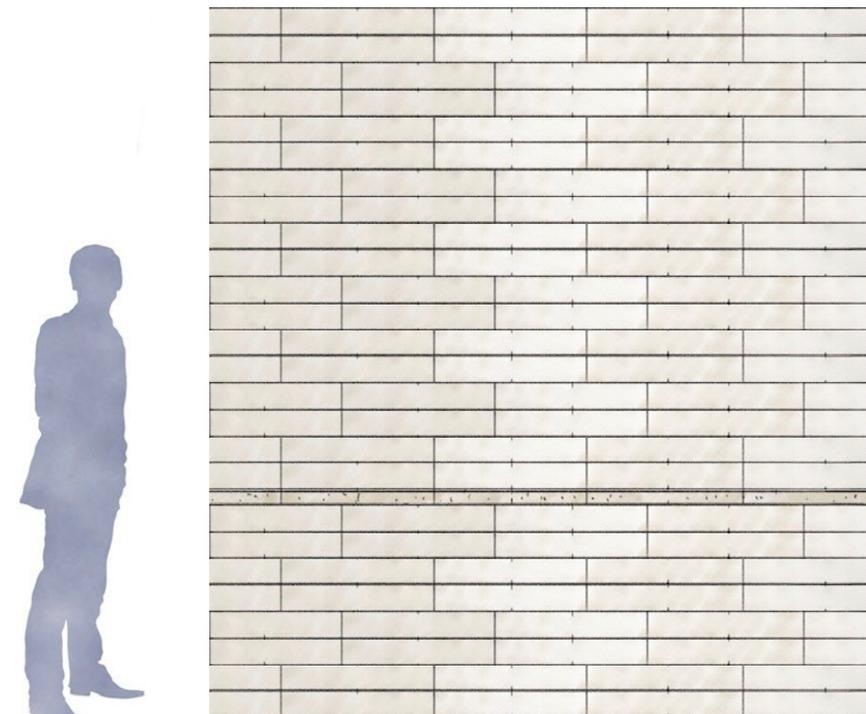


Raw

“Kyle Cut” Limestone Design



Kyle Cut - Study 1 - varying sizes and textures



Kyle Cut - Study 2 - 4" paired stacked bond with textured 2" band

The paired running bond symbolizes two tracks running in sync, a nod to the railway, the reason Kyle was established here in 1880. The parallel masonry also represents “Old” and “New”, two strong ideas that are embraced and work in unison throughout these design standards.

Concrete Palette



Natural concrete

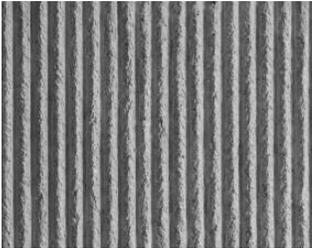


Stained concrete (natural colors)

Concrete Texture



Board formed



Ribbed Concrete



Smooth

Accent Materials:



Hot-Rolled Steel

Applications:

- Exterior metal work
- Structural elements
- Interior feature walls
- Canopies



Wood

Applications:

- Exterior cladding (composite material acceptable as deemed by City Architect)
- Interior feature walls
- Storefronts
- Canopies



Mass Timber

Applications:

- Structure
- Incentives provided by city for use



Brick Details:

Building Top/Cornice



Rotated Soldier Course



Alternating Header Course

Middle/Field Brick



Paired Running Bond

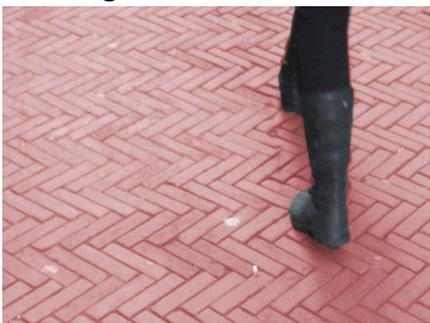


Pulled brick



Pulled brick

Building Base/Walk Surface



Herringbone



Basketweave



Running Bond

Mortar Colors: Complimentary and matching brick

Mortar Joints: Tooled, Flush, German Schmear

Surface Parking Materials

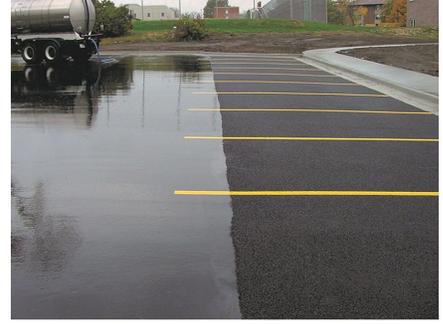
Porous Pavement



Pervious Concrete



Pervious Asphalt and Concrete Pavers



Normal Asphalt (left), Porous Asphalt (Right)

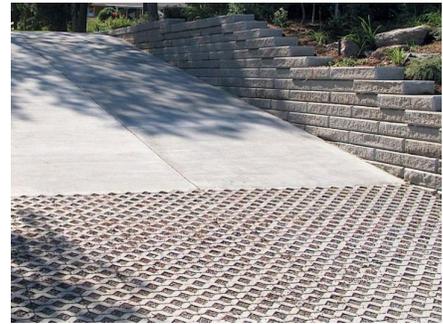
Alternative Pavers



TRUEGRID™ with Crushed Granite Infill



TRUEGRID™ with Clean Limestone Infill



UNILOCK Turfstone™



Gravelpave² with various infills



Checker Block™ with grass infill



Grasspave²

Materials: Focus Areas

Downtown Core / CBD

- Primary materials: masonry (limestone or brick)
- Secondary materials: smooth faced stucco, light colored precast concrete, metal panel
- Accent materials: decorative metals, wood, mass timber and similar high-quality materials reviewed by the City Architect/ P&Z and deemed to be compatible/ complimentary.
- High quality storefront glazing systems with VTR (Visual Transmittance Rating) of .6 or higher. Minimum storefront to building wall ratio of 75% along street fronts on ground level from grade to 8'-0" above the pedestrian walkway.
- Projecting awnings and/or canopies that provide color and rain protection to be approved by City Architect.

Downtown Core facing Mary Hartson Park

- Primary materials: masonry (limestone or brick)
- Secondary materials: masonry (limestone or brick)
- Accent materials: decorative metals, wood, mass timber and similar high-quality materials reviewed by the City Architect/ P&Z and deemed to be compatible/ complimentary.
- High quality storefront glazing systems with VTR (Visual Transmittance Rating) of .6 or higher. Minimum storefront to building wall ratio of 75% along street fronts on ground level from grade to 8'-0" above the pedestrian walkway. Storefront system color to be dark color.
- Projecting awnings and/or canopies that provide color and rain protection to be approved by City Architect.



Watkins Building, Wang Architects

I-35 Corridor

- Primary materials: masonry, smooth faced stucco, light colored precast concrete, metal panel
- Secondary materials: smooth faced stucco, light colored precast concrete, metal panel, wood
- Accent materials: quality materials reviewed by the City Architect/P&Z and deemed to be compatible/complimentary.
- High quality storefront glazing systems with VTR (Visual Transmittance Rating) of .6 or higher. Minimum storefront to building wall ratio of 25% along street fronts and main facades as deemed by City Architect on ground level from grade to 8'-0" above the pedestrian walkway/grade.

I-35 Corridor (within Downtown Core)

- Primary materials: masonry (limestone or brick)
- Secondary materials: smooth faced stucco, light colored precast concrete, metal panel
- Accent materials: decorative metals, wood, mass timber and similar high-quality materials reviewed by the City Architect/P&Z and deemed to be compatible/complimentary.
- High quality storefront glazing systems with VTR (Visual Transmittance Rating) of .6 or higher. Minimum storefront to building wall ratio of 50% along street fronts and main facades as deemed by City Architect on ground level from grade to 8'-0" above the pedestrian walkway/grade.
- Projecting awnings and/or canopies that provide color and rain protection to be approved by City Architect.



Fresh Street Market, Chandler Associates Architecture Inc.

Materials: Focus Areas

Trail-Oriented Development & Vybes

- Primary materials: wood and mass timber
- Secondary materials: masonry, smooth faced stucco, light colored precast concrete, metal panel
- Accent materials: quality materials reviewed by the City Architect/P&Z and deemed to be compatible/complimentary.
- High quality storefront glazing systems with VTR (Visual Transmittance Rating) of .6 or higher. Minimum storefront to building wall ratio of 50% along street fronts on ground level from grade to 8'-0" above the pedestrian walkway.
- Projecting awnings and/or canopies that provide color and rain protection.



Press Café in Fort Worth, Texas by EDG Design

Kyle Suburbs

- Residential developments are to be evaluated on a case by case basis.
- Acceptable exterior materials are fiber cement siding, masonry, wood, smooth faced stucco and metal panel.
- Unacceptable materials include: faux brick, faux stone veneer panels, plastic and vinyl siding.



Residential Single-Family Home Concept, BGK Architects

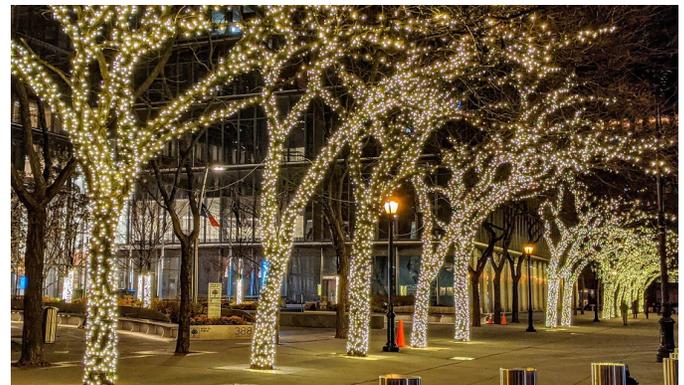
1.11 STREET LIGHTING

Lighting is a key element in establishing the feeling of a place. The City of Kyle established a Lighting Ordinance (Ordinance No. 909) in 2016 that thoroughly detail guidelines for new lighting installations, as well as how to address existing lighting that does not conform with the new standards. Additionally, the city has chosen and accepted a lighting fixture series, the DS770FC series, Dark Sky Acorn, by Amerlux*.

Street light spacing should be respectful of tree spacing, with a minimum of 10 feet between the base of the streetlamp and the trunk of the closest tree.



Full Cutoff Parking Lamps



Seasonal Tree Lighting

Acceptable Lighting Types & Styles



Festoon Lights



Fully-Shielded Recessed Wall Fixtures



Full Cutoff™ Ground Fixtures



Flush-Mounted Canopy Fixtures

1.12 PUBLIC ART

As the city experiences changes in its built environment, residents may face challenges relating to construction process and infrastructure improvements. These long-term processes can prove burdensome if forward momentum isn't maintained, which can prove especially difficult for longer projects.

Public Art initiatives are a fun way to maintain excitement for the city's growth and to keep community members actively involved in the design of their city. There are a variety of initiatives that can be implemented, such as Traffic Signal Boxes wrapped in vinyl printed with artists' work, commissioned murals, and public pop-up spaces throughout the city for artists to host and sell their work.

Traffic Signal Boxes

The City of Kyle invites local artists, children and teens to beautify the urban environment with their creativity. Traffic Signal Boxes are preexisting fixtures in the urban environment - by adorning them, this commonplace item is able to add another layer of vibrancy to its context, allowing artists in the community to affect their landscape.

The vinyl wraps used on the signal boxes lasts for a minimum of 2 years, and much longer as long as the fixture is not damaged, providing a cost-effective way to generate buzz and beautify the city without disrupting residents with construction processes.



"Water Goddess", Eric Lothspeich



"Monarchs and Bluebonnets", Pamela Summers

Murals

The City of Kyle is excited for the opportunity to collaborate with local artists on murals for restorative projects and new construction. For selected properties in existing neighborhoods, the city will hold calls for submissions for neighborhood artists to add a mural that speaks to that community. For new construction, the city welcomes local artists to submit their design to guide the future aesthetics of Kyle. Businesses are also encouraged to commission local artists for murals.



"Mary Smith Bailey", Devon Clarkson



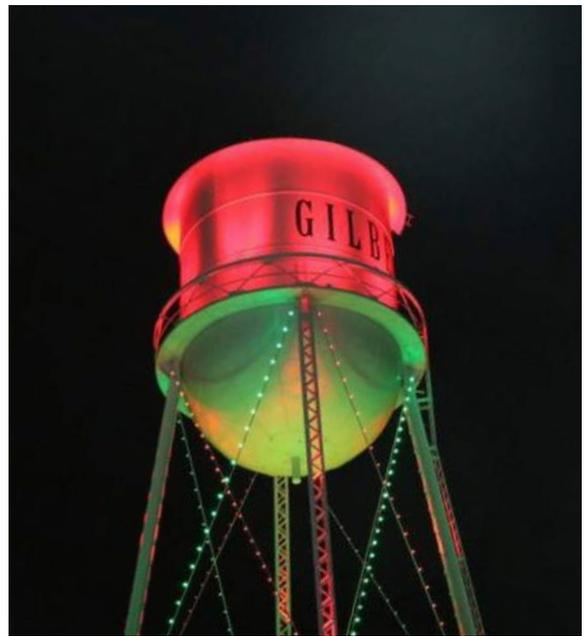
"Jumping Around", Joe Magnano

Other Opportunities

Additional seasonal / temporary downtown opportunities include: redesigned crosswalks, lighting the water tower, illuminating the Kyle Creamery.



Painted crosswalks



Variable color LED water tower lighting

1.13 SERVICES AND UTILITIES

Switch Gear and Electric Transformers

Switch gear and transformers should generally be located on tertiary streets and service corridors/alleyways. If the equipment must be on a secondary street, they must be substantially setback from pedestrian corridors and screened with appropriate materials (man-made and/or landscaped with an accessible path to the device for maintenance), or decorated with commissioned public art. In general, switch gear, transformer pads, and other equipment should not be visible from or located on primary streets and sidewalks, especially those that are focused on pedestrian use.

Reduced Pressure Zone (RPZ) Devices

RPZ's shall be screened by encasing in an enclosure that is recommended by the manufacturer.

Grease Traps

Grease traps shall be carefully located and are not permitted along primary streets and their sidewalks, especially those that are focused on pedestrian use. They should be located within service zones or in setback areas along tertiary streets and/or alleys.

Trash and Recycling Receptacles

All trash and recyclables generated by a business shall be disposed of by that business in receptacles provided within the building or within the exterior space used by the building for seating, dining, advertisement, etc.,. Any exterior receptacles shall be made of durable, commercial grade materials and located against the building facade. These receptacles will not be permitted along the curb.



Lasercut Metal Gate for Utilities at Lancaster Mall, Weldcraft



Hedge screening Transformer Box from view



Hedge screening Transformer Box with adequate space between landscaping and box for maintenance

1.14 ENVIRONMENTAL CONSIDERATIONS

Emerald Crown Trail

The Emerald Crown Trail is a proposed trail network connecting the Central Texas cities of San Antonio, New Braunfels, San Marcos, Kyle, Buda, and Austin. It would consist of a variety of trail types, amenities, and link to adjacent parks and sites, and would tie into the Great Springs Project, a “...national park-scale corridor of protected lands between Austin and San Antonio and a regional network of trails that link Barton Springs, San Marcos Springs, Comal Springs, and San Antonio Springs” (San Marcos Greenbelt Alliance).

The Vybe

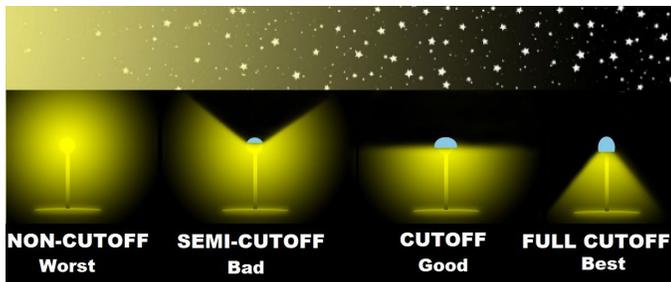
“The Vybe” is a proposed trail system across Kyle that will provide access to the greater Emerald Crown Trail and will interconnect proposed “Vybes”. These Vybes will be distinctive developments that offer attractions, eateries, and entertainment and reflect their local context; for example, a more “natural” portion of The Vybe may host a bike shop and cafe, while a sports-themed Vybe would be adjacent to an athletic complex.

The Vybe will be constructed with 12’ in width to accommodate golf carts and other small vehicles. Additionally, it will span several environments present in Kyle such as urban, natural, wooded, residential, and commercial, offering residents the opportunity to explore the city without a vehicle.

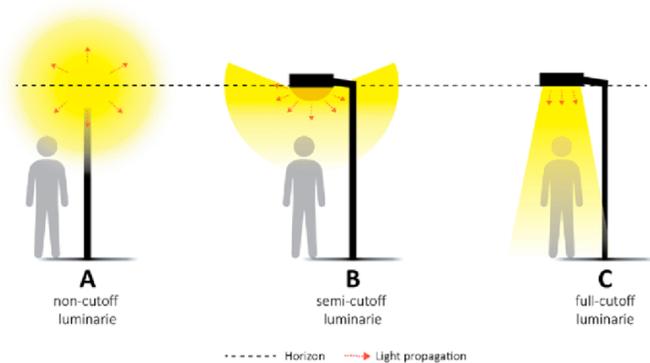


Green Building Standards

- No uplighting of facades; require cutoff light fixtures on sites within city limits.
- Bird-safe glass for any structure w/glazing above 2 stories, excluding Kyle Suburbs
- Locally-sourced materials (sourced within 500 miles of city limits) are suggested for all new projects in Downtown Core, I-35 Corridor, and TODs, and housing developments.
- Consider tax and other incentives to residents for using locally-sourced materials in remodels and/or new builds.
- Native plantings required in Downtown Core, I-35 Corridor, and TODs. Native plantings recommended for Kyle Suburbs, but non-native, non-invasive species allowed. Planting invasive species is strictly prohibited in any new landscaping.
- No turf grass at Vybes, Trails, or Downtown Core. Turf grass is permitted in all other spaces, but irrigation systems must be drip systems - no new sprinkler systems permitted.
- Heat Island reduction: in the Downtown Core, I-35 Corridor, and TODs, new roofs will need a minimum Solar Reflective Index (SRI) of 78 over at least 75% of the roof's surface for roofs with slopes less than 1:12.
- LEED Certification required for all new projects in I-35 Corridor (minimal rating, can exceed). LEED Silver rating required for all new projects across from Downtown CBD. LEED Buildings aim to reduce their contribution to global climate change, protect water resources and local ecosystems, promote sustainable material cycles, and enhance the community's quality of life.



Cutoff Conditions for Ample Night Sky Protection



Variety of Lighting Cutoff Conditions


LEED Certified
40-49 points


Silver Certification
50-59 points


Gold Certification
60-70 points


Platinum Certification
80+ points

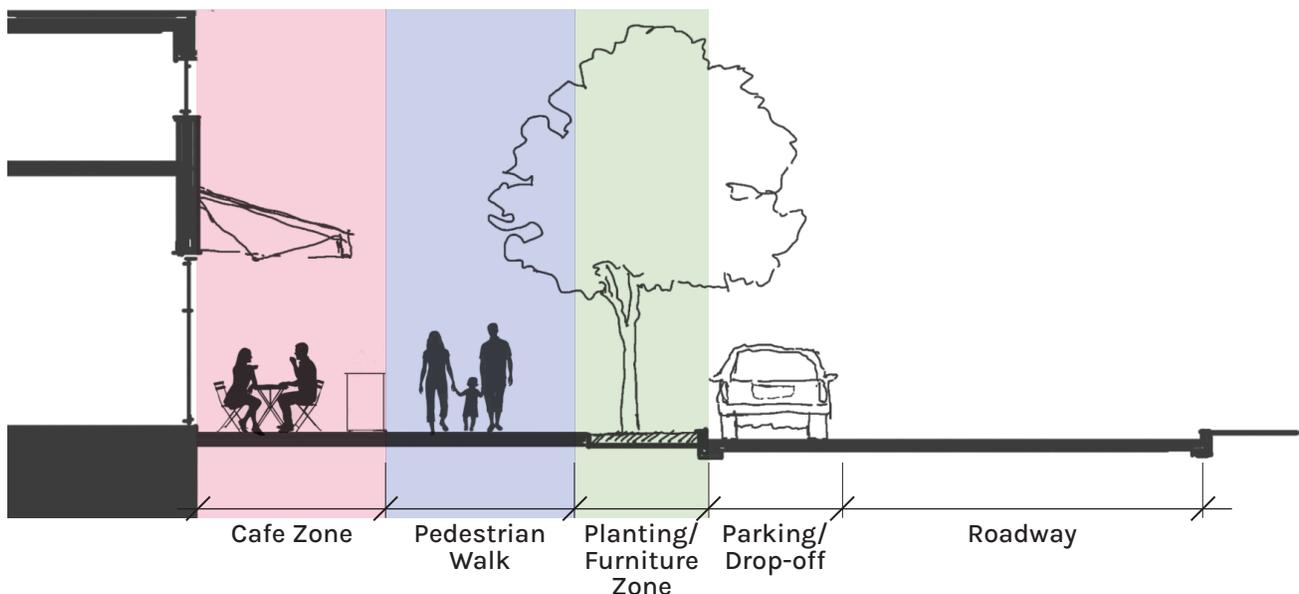
LEED Certification Ratings

1.15 PEDESTRIAN EXPERIENCE

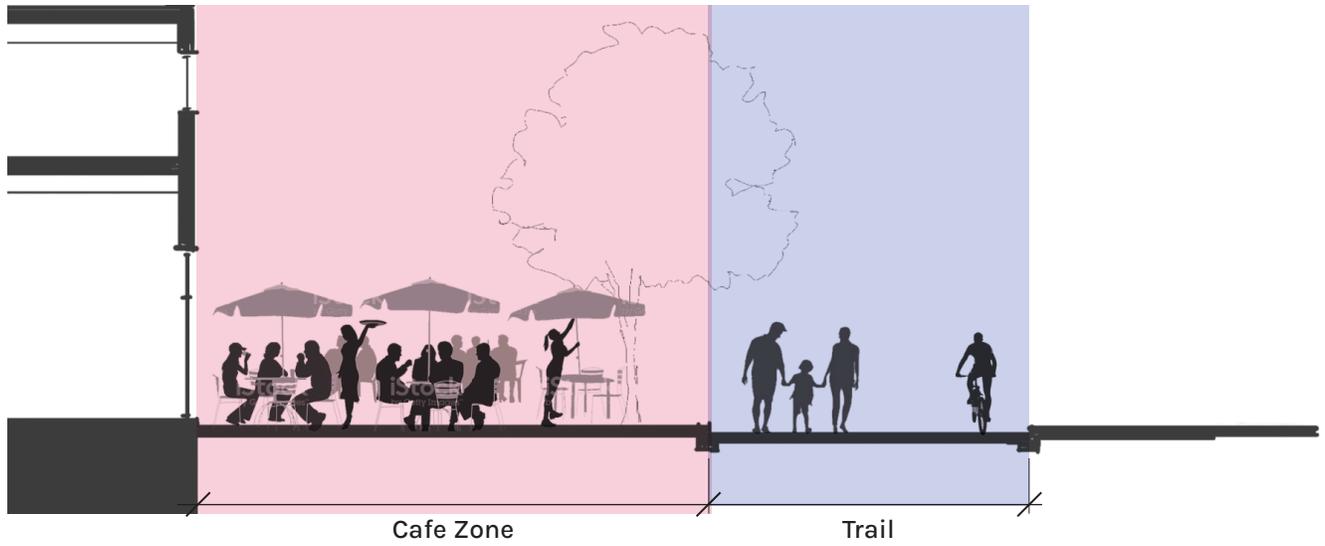
Pedestrian-Focused Design Principles

- Create a hierarchy between public spaces and the roadway, giving priority to public spaces. Wherever possible, create larger sidewalks, landscaping opportunities, and gathering spaces, giving the roadway the minimum amount of space necessary to operate.
- Give priority to tenants with publicly-accessible businesses in ground level leases. These can include restaurants/eateries, commercial tenants, art/music venues, and other establishments that generally serve the public.
- Maintain ADA-compliant sidewalks and thoroughfares, always making the greatest effort to keep outdoor pedestrian surfaces well-maintained for ease of use with mobility aids. Maintain a communication line that is frequently checked and well-publicized for citizens to report maintenance concerns of walking surfaces so that the pedestrian experience is always widely accessible.
- Use quality materials and heightened attention to detail on ground floors to craft an enhance space along pedestrian thoroughfares.
- Building massing and entry points should be articulated at street corners to define and enhance the intersection.
- Where possible, recess building entrances for visual variety and to avoid door swings in the sidewalk pathway.
- Reinforce the connection to the pedestrian and street front with heightened articulation of ground floor elements.
- Primary building entrances should be accentuated.

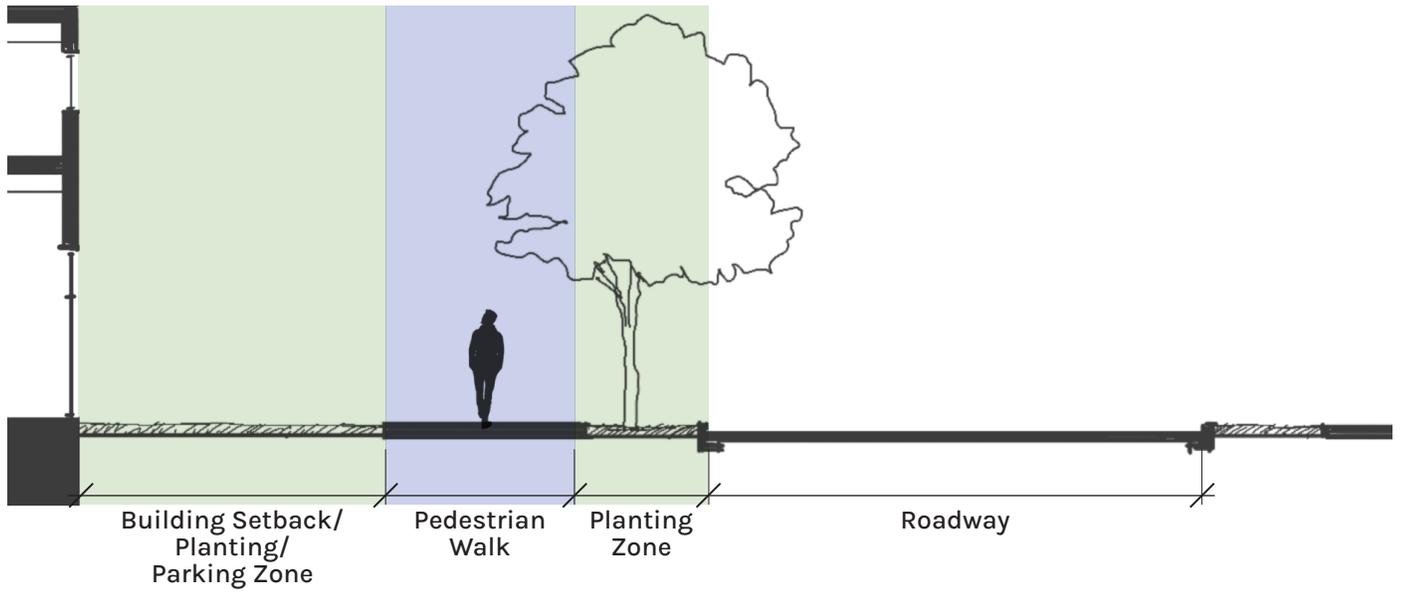
Downtown CBD



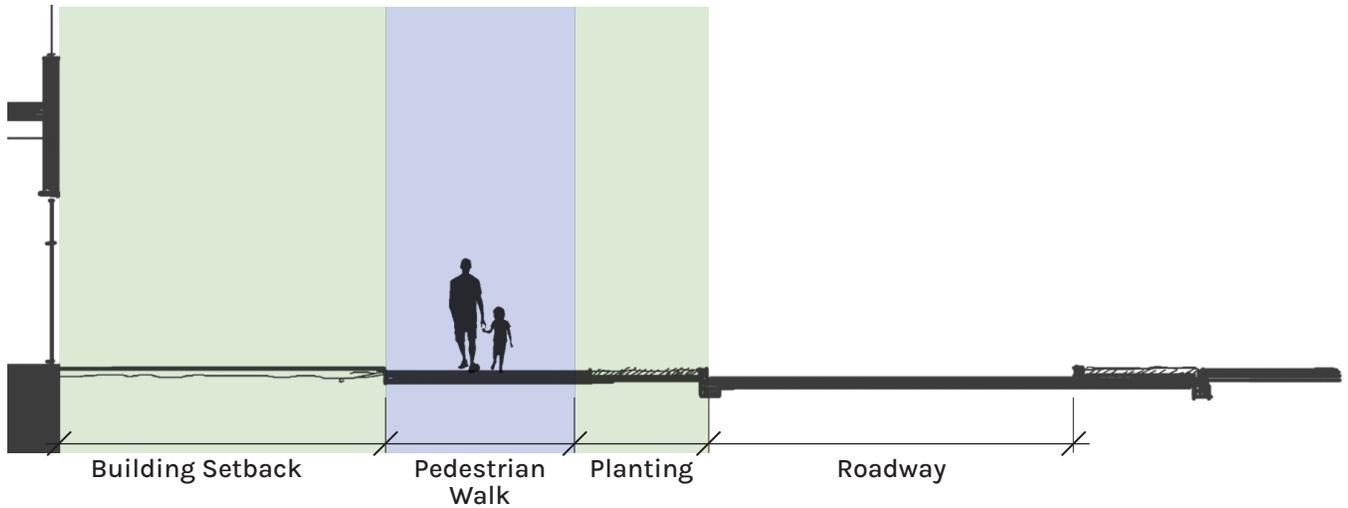
Trail Oriented Development



I-35 Corridor



Kyle Suburbs



Trees

Trees are an ideal way to effectively provide shading, reduce heat-island effects in urban areas, provide homes for wildlife, and enhance the quality of outdoor spaces for citizens. Native trees should compose the majority of new plantings, and invasive species are prohibited.

To ensure that planned trees will thrive in their site, it is recommended that the Kyle Parks and Recreation department and a City Arborist for Kyle are consulted for prior approval. Additionally, it is recommended that this department oversees tree plantings, maintenance, and preservation.

Each Focus Area will have a different character and environment, and so the trees and foliage planted at each will have to be curated to match the needs of each site. Generally, requirements will vary across:

- Mary Kyle Hartson Park
- Burleson
- Front Street
- CBD
- I-35 Corridor
- TOD
- Kyle Suburbs



Cedar Elm

A hardy deciduous tree that makes is ideal for the urban environment. Suggested placements are along streets in Downtown Core and I-35 Corridor.



Carolina Cherry Laurel

A smaller, dense tree that blooms white flowers and great for screening. Suggested placements are in front of urban residences and between public spaces and neighborhoods.



Texas Mountain Laurel

Small tree with several trunks and fragrant purple flowers. Makes for a fantastic signifier of Spring beginning, and suggested for Vybes, plaza spaces, and trailheads.



Escarpment Black Cherry

Blooms white flowers in Spring, and leaves turn bright gold in Fall. Suggested around TODs and Vybes, as well as outdoor areas that host seasonal events, such as Mary Hartson Park.



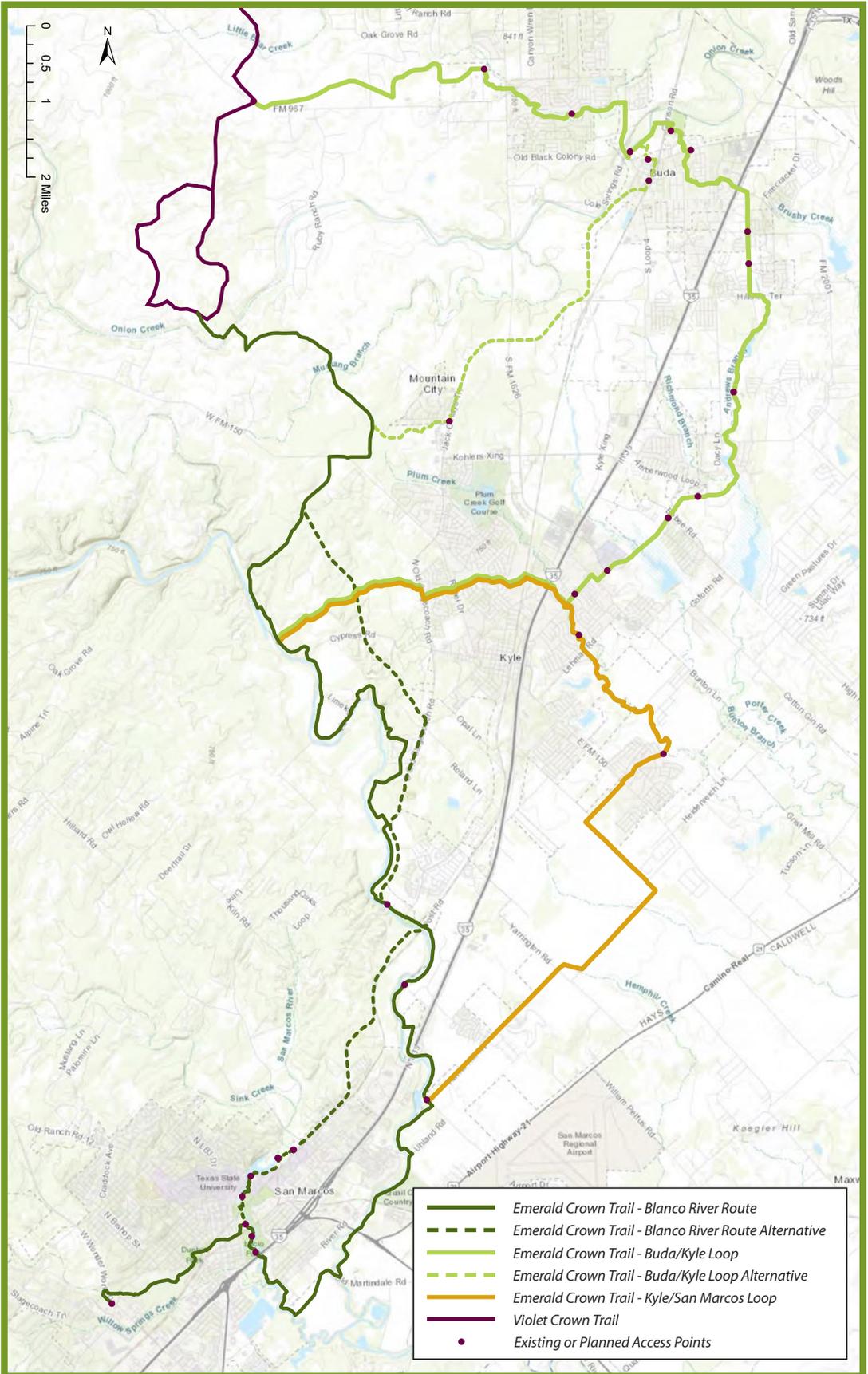
Yaupon Holly

Adaptable to different soils and keeps leaves throughout the year, with red berries seasonally. Responds to pruning and is great for screening. Suggested as smaller-scale landscaping.



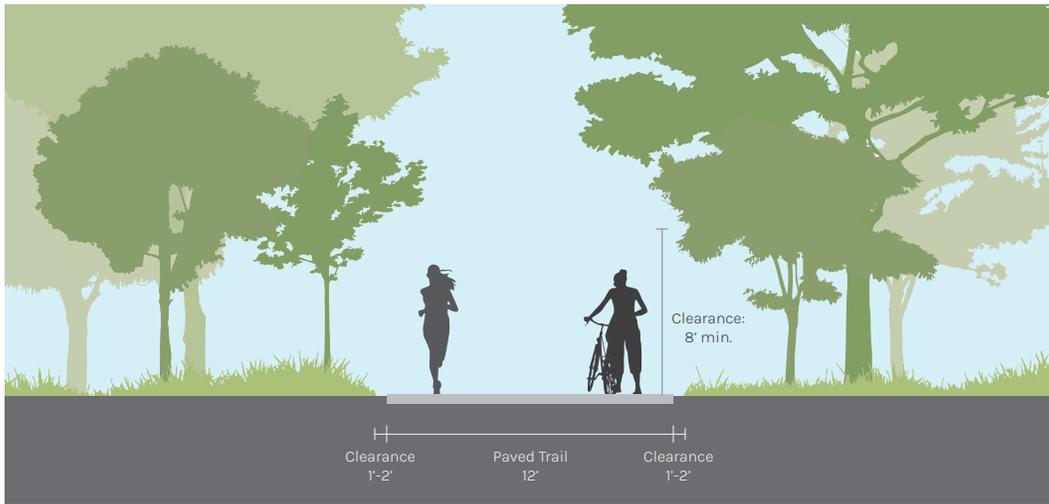
Gum Bumeria

Provides nourishment for birds, white-tailed deer, and honey bees. Suggested placements just outside dense areas to support wildlife, as well as around TODs and Vybes.



Emerald Crown Trail

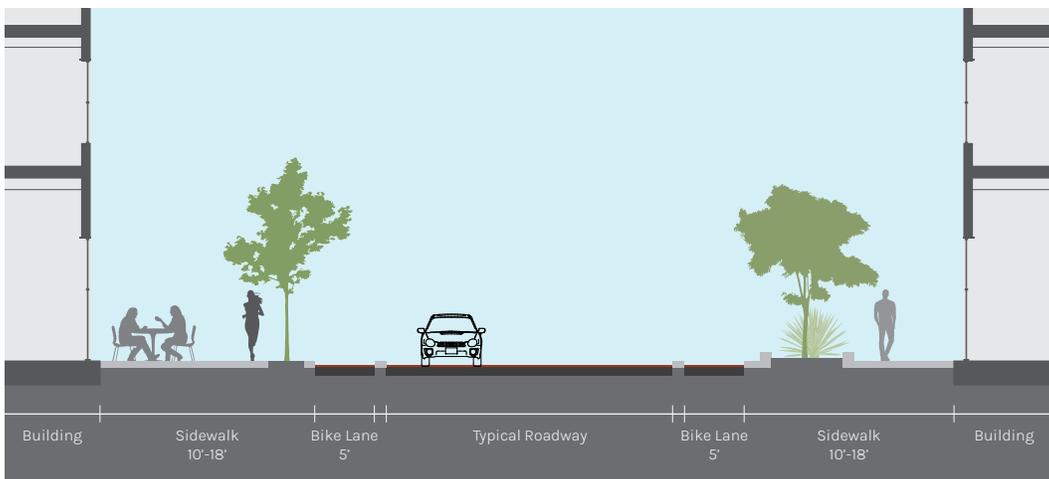
Trail Conditions



Typical Trail Condition



Burleson North/Street-Adjacent Trail Condition



Center + Burleson Street/Vybe-Adjacent Trail Condition



**DESIGN
STANDARDS
APPENDIX**

1.15 CITY POLICY ACTION ITEM TRACKING

Action Step		Lead Agency	Potential Partners	Resources & Actions Required	Potential Funding Sources	Timeframe
City Policy / Regulation Action Item						
1	Review and modify zoning to ensure office development is favored at office priority sites; residential at residential priority sites; etc. Allow temporary pop-up uses; Create minimum density standards as well as maximum; adjust the zoning in identified areas of opportunity for increased intensity.	P&Z	Economic Development	Planning Code amendment and Zoning Map amendment		Near-Term
2	Review and modify zoning to promote density and a mix of transit-supportive uses at specified locations along planned and existing trail system, and potential future commuter rail locations.	P&Z		Planning Code amendment and Zoning Map amendment		Near-Term
3	Create "Vybe" or Trail-oriented development zoning categories	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
4	Review and modify zoning to expand Central Business District Zones CBD-1 and CBD-2	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
5	Incentivize the use of privately-owned, vacant, or underutilized buildings as temporary art or cultural space	Mayor's Task force		Evaluate and update building code and requirements		Immediate
6	Establish requirements for future live-work spaces to ensure they are not used solely as residences, beyond the existing requirement of having a business license.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
7	Allow rooftop open spaces.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
8	Establish temporary activity permits for pop-ups.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
9	Establish new arts & culture land use category.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
10	Consider regulations to promote a diversity of businesses downtown, with incentives for basics services and health-centric goods and services	P&Z	Economic Development	Planning Code amendment and Zoning Map amendment		Near-Term
11	Incorporate design guidelines that require new development compatibility with surrounding context. Include a method to enforce and/or incentivize compliance.	P&Z		Planning Code amendment and Zoning Map amendment		Near-Term
12	Establish a temporary use classification.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
13	Reduce regulatory barriers to outdoor vendors in downtown, particularly within parks, public gathering spaces, and pop-up spaces.	P&Z	Economic Development	Planning Code amendment and Zoning Map amendment		Immediate
14	Consider allowance for extended hours of operation including allowances for noise in outdoor public spaces Downtown.	P&Z		Planning Code amendment and Zoning Map amendment		Immediate
15	Update signage ordinance to promote cohesive and attractive retail signage in all areas	P&Z		Planning Code amendment and Zoning Map amendment		Near-Term

1.16 SIGNAGE ORDINANCE MATRIX

District	Illumination Type				Brightness Limitation for Digital Displays and Message Center Signs	Hours of Illumination	Motion Limitation	Size Limitation	
	Internal	Message Center Sign	External	Digital Display				Digital Displays and Message Center Signs	Digital Display Signs as a Max % of Total Sign Area on Site
Ag/Rural	N	N ^{^^^}	N ^{^^}	N	N/A	N/A	N/A	N/A	N/A
Residential	N	N	Y	N	N/A	N/A	Determined by visibility. See §6.f.4.c	N/A	50%
Institutional	Y	Y	Y	N	Daytime: 5,000 Nits Nighttime: 250 Nits	5 am to 11 pm or 1/2 hour past close of business	Determined by visibility. See §6.f.4.c	N/A	50%
Main Street	Y	N [^]	Y	N	N/A	5 am to 11 pm or 1/2 hour past close of business	Determined by visibility. See §6.f.4.c	N/A	50%
Village Commercial	N	N	Y	N	N/A	5 am to 11 pm or 1/2 hour past close of business	N/A	N/A	N/A
General Commercial & Industrial	Y	Y	Y	Y	Daytime: 5,000 Nits Nighttime: 250 Nits	5 am to 11 pm or 1/2 hour past close of business	Determined by visibility. See §6.f.4.c	30%	50%
Off- Premises*	Y	Y	Y	Y	Daytime: 5,000 Nits Nighttime: 250 Nits	5 am to 11 pm or 1/2 hour past close of business	Determined by visibility. See §6.f.4.c	100%	100%
Temporary Signs*	N	N	N	N	N/A	N/A	N/A	N/A	N/A
Portable Signs*	N	N	N	N	N/A	N/A	N/A	N/A	N/A

*Off-premises, temporary, and portable signs are subject to the illumination regulations governing off-premises, temporary and portable signs, rather than the illumination standards governing the specific district where the sign is located

[^] Excludes marquee signs

^{^^} Excludes signs located in Parks or Recreational Facilities

^{^^^} Excludes scoreboards located in Parks or Recreational Facilities

1.17 PREFERRED BRICK SIZING



AVAILABLE SIZES & SPECS

EFFECTIVE JUNE 2021

MODULAR SIZE			
	FULL SIZE BRICK	PIECES PER S.F.	6.5 PC
		PIECES PER PALLET	860 PC
		PIECES PER TRUCKLOAD	12,040 PC
		WEIGHT PER PIECE	3.85 LB
	THIN CUT BRICK	PIECES PER S.F.	6.5 PC
		PIECES PER L.F.	4.5 PC
		PIECES PER PACKAGE	PACKAGING IS DONE TO THE EXACT AMOUNT REQUESTED
		WEIGHT PER FLAT	0.95 LB
		WEIGHT PER CORNER	1.35 LB

QUEEN SIZE			
	FULL SIZE BRICK	PIECES PER S.F.	5.5 PC
		PIECES PER PALLET	948 PC
		PIECES PER TRUCKLOAD	12,324 PC
		WEIGHT PER PIECE	3.70 LB
	THIN CUT BRICK	PIECES PER S.F.	5.5 PC
		PIECES PER L.F.	4 PC
		PIECES PER PACKAGE	PACKAGING IS DONE TO THE EXACT AMOUNT REQUESTED
		WEIGHT PER FLAT	1.25 LB
		WEIGHT PER CORNER	1.40 LB

MID CENTURY 12"			
	FULL SIZE BRICK	PIECES PER S.F.	6 PC
		PIECES PER PALLET	860 PC
		PIECES PER TRUCKLOAD	11,180 PC
		WEIGHT PER PIECE	4.15 LB
	THIN CUT BRICK	PIECES PER S.F.	6 PC
		PIECES PER L.F.	6 PC
		PIECES PER PACKAGE	PACKAGING IS DONE TO THE EXACT AMOUNT REQUESTED
		WEIGHT PER FLAT	1.10 LB
		WEIGHT PER CORNER	1.40 LB

PAVER MODULAR SIZE			
	FULL SIZE BRICK	PIECES PER S.F.	4.5 PC
		PIECES PER PALLET	900 PC
		PIECES PER TRUCKLOAD	11,700 PCS
		WEIGHT PER PIECE	4.0 LB
	THIN CUT BRICK	PIECES PER S.F.	4.5 PC
		PIECES PER L.F.	N/A
		PIECES PER PACKAGE	PACKAGING IS DONE TO THE EXACT AMOUNT REQUESTED
		WEIGHT PER FLAT	1.70 LB

1.18 18-HOUR CITY INFORMATION

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DEFINING 24-HOUR AND 18-HOUR CITIES, ASSESSING THEIR VIBRANCY, AND EVALUATING THEIR PROPERTY PERFORMANCE

Executive Summary. Indicators originally used to define 24-hour cities are updated to redefine 24-hour cities in the United States. From the sample of 42 large cities, we identify six 24-hour cities and nine 18-hour cities. The six 24-hour cities (Tier I), nine 18-hour cities (Tier II), and 27 9-to-5 cities (Tier III) are compared. For office properties, investment performance indicators correspond in rank order to Tiers I–III. For apartments, however, the results are less consistent. Dislocations in housing markets over the past decade have prompted a notable investor preference for multifamily investment in Tier II and Tier III markets during the recovery from the Global Financial Crisis.

**Hugh Kelly
Emil Malizia**

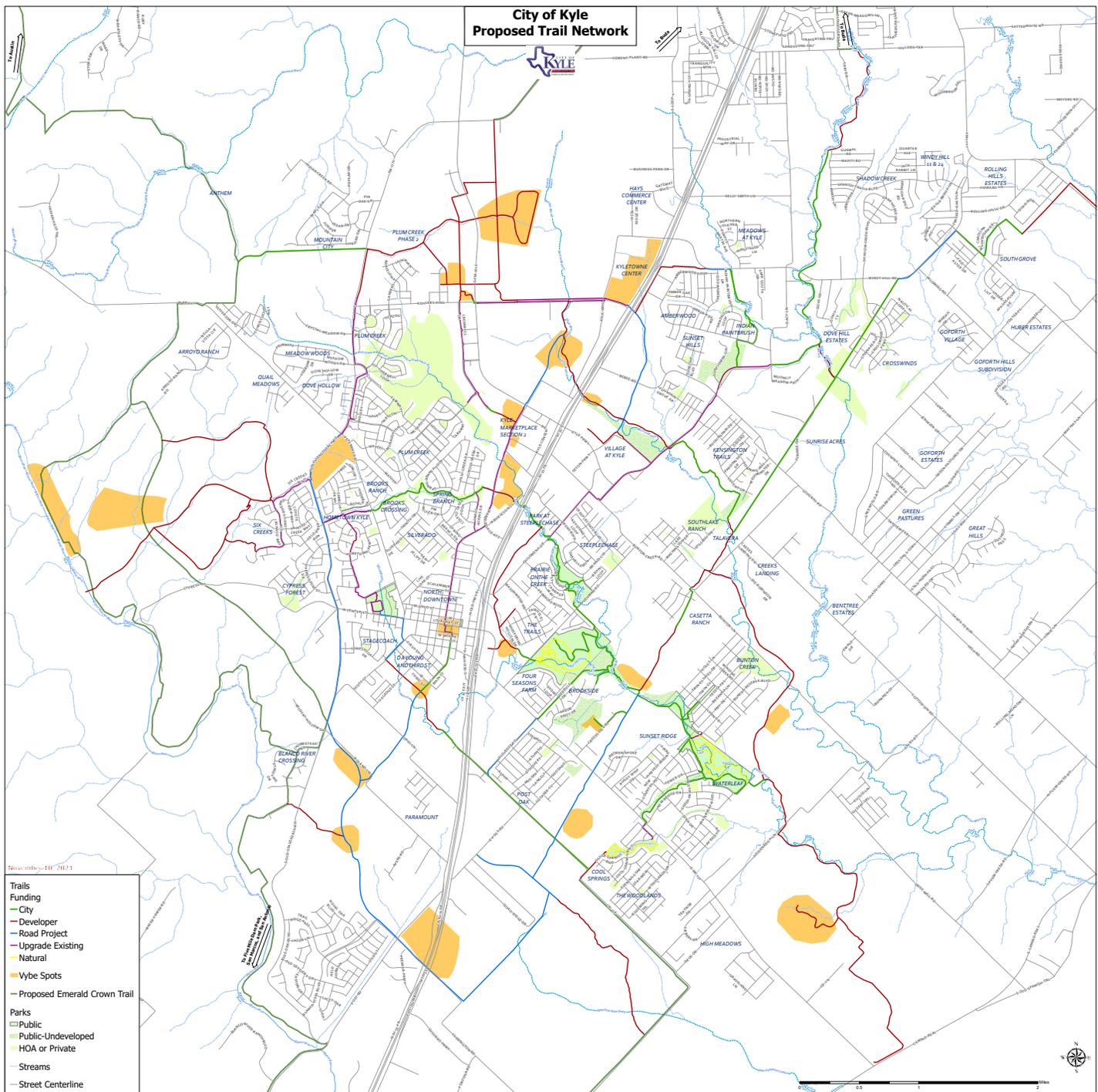
We develop rigorous definitions of 24-hour and 18-hour cities in the United States and distinguish them from 9-to-5 cities. We begin with six indicators, five of which Kelly (2011) previously studied for 14 large U.S. cities in 2007. We expand the analysis to a larger group of 42 cities examined in Malizia's (2015) research on vibrant centers. Our objectives are to advance the discussion of cities beyond the level of descriptive and largely subjective considerations to measurable attributes amenable to quantitative analysis, and to relate the results of that analysis to performance measures useful for real estate investors concerned with investment property and public policymakers seeking a more viable tax base.

In the next section, we summarize the Urban Land Institute's (ULI's) discussion of 18-hour cities and then apply Kelly's (2011) criteria. We compare the resulting categorization of 24-hour city or 18-hour city to vibrancy scores for the downtowns of these 42 places. We establish groupings with these criteria and with K-means cluster analysis. Finally, we use property performance metrics to determine whether six 24-hour cities (Tier I), nine 18-hour cities (Tier II), and 27 9-to-5 cities (Tier III) perform distinctively.

1.19 ZONING DISTRICT REFERENCE CHART

Abbreviated Designation	Zoning District Name	Designation
A	Agricultural district	District A
UE	Urban estate district	District UE
R-1-1	Single-family residential 1	District R-1-1
R-1-2	Single-family residential 2	District R-1-2
R-1-A	Single-family attached	District R-1-A
R-1-T	Residential townhouse	District R-1-T
R-1-C	Residential condominium	District R-1-C
R-2	Residential two-family	District R-2
R-3-1	Multifamily residential 1	District R-3-1
R-3-2	Multifamily residential 2	District R-3-2
R-3-3	Apartments residential 3	District R-3-3
M-1	Manufactured home	District M-1
M-2	Manufactured home subdivision	District M-2
M-3	Manufactured home park	District M-3
CBD-1	Central business district 1	District CBD-1
CBD-2	Central business district 2	District CDB-2
RS	Retail/service	District RS
W	Warehouse	District W
CM	Construction/manufacturing	District CM
E	Entertainment	District E
TU	Transportation/utilities	District TU
B	Billboards	District B
RV	Recreational vehicle park	District RV
HS	Hospital services	District HS
PUD	Planned unit development	District PUD
H	Historic district overlay	District H overlay
CU	Conditional use overlay	District CU overlay

1.20 PROPOSED VYBE TRAIL SYSTEM



Proposed Trail Network Map

1.21 DESIGN STANDARDS REVIEW PROCEDURE

Procedure

Applicant to provide all necessary materials to the Design Review Committee or City Architect. Refer to the city's website for current instructions.



January 20, 2022

City of Kyle
Development Services
Design Review Process

Introduction

For all projects planned in the City of Kyle, the project owner will be required to adhere to a three-stage submission process as outlined below:

Preliminary Site Plan Approval

Preliminary layout and base information must be approved by the City Architect and City Planner prior to completing design documents. Documents should include:

- Conceptual Site Plan at 1"=20', indicating building footprints, rights-of-way, lot lines, setbacks, streets, driveways, alleys, parking layouts, service areas, proposed utility connections and publicly accessible open spaces.
- Development Program and Key Information including, building floor area, the anticipated mix and number of dwelling units, covered and uncovered parking spaces, bicycle parking, open space area, impervious cover for each lot, building heights, residential and commercial density, and other key information that is warranted by the particular project.
- Conceptual Grading Plan indicating existing contours from the mass grading plan, proposed changes to lot grades, overall site drainage and proposed ground level finished floor elevations.

All plans and drawings to be submitted in electronic 11"x17" pdf format.

Architectural Design Approval

Following the approval of the preliminary site plan, portions of the architectural design must be completed and submitted to the City Architect for approval. Any changes to the preliminary site plan will also be presented to the City Architect for their approval as a proposed amendment to the approved preliminary site plan.

The architectural design submittal must include:

- Narrative including list of changes from Preliminary Site Plan.
- Site Plan at 1"=20', indicating building footprints, rights-of-way, lot lines, setbacks, streets, driveways, alleys, parking layouts, service areas, furnishings, proposed utility connections and publicly accessible open spaces.
- Updated Development Program and Key Information.
- Floor Plans at 1/16"=1'-0" or greater, describing the configuration of ground floor and upper floors for each building, with clear indication of parking, service areas and mechanical rooms, dwelling units, common open space, porches, etc.
- Building Elevations and Sections at 1/8"=1'-0" or larger of each elevation, describing key architectural elements, proposed building materials, colors and finishes.
- Illustrative Renderings describing the overall character and specific features of the public environment and the proposed buildings.
- Landscape Plan at 1"=20' or larger, showing the location and type of plant materials, hardscape, fences, walls, furnishings, etc.
- Material Sample Board indicating exterior colors and materials to be used for each building and a roofing material sample.
- Grading Plan indicating existing contours from the mass grading plan, proposed changes to lot grades, overall site drainage and finished floor elevations.

All plans and drawings to be submitted in electronic 11"x17" pdf format.

Final Approval

An applicant may not commence construction of any improvement until it obtains written approval from the City Architect. The final approval submission must include:

- Narrative including list of any changes from the approved architectural design documents listed above.
- Approved Construction Documents including a complete set of construction drawings approved by the City of Kyle and final specifications.

All plans and drawings to be submitted in electronic 11"x17" pdf format.

Governmental Requirements

To the extent that any applicable government ordinance, building code or regulation imposes a more restrictive standard than the recommendations provided by the City Architect, the applicable government standard will control. To the extent that any applicable government standard is less restrictive, recommendations provided by the City Architect will control. Plans submitted to the City Architect must comply with all applicable laws, codes, regulations, and governmental requirements.

Limitation of Liability

Approval of plans and specifications is not a representation, warranty or guarantee that the structure is in compliance with governmental requirements or restrictions or requirements. Please be advised that City Preferences may evolve due to economic, social, and other factors.



DOWNTOWN MASTER PLAN

December 2022



Barnes
Gromatzky
Kosarek
Architects

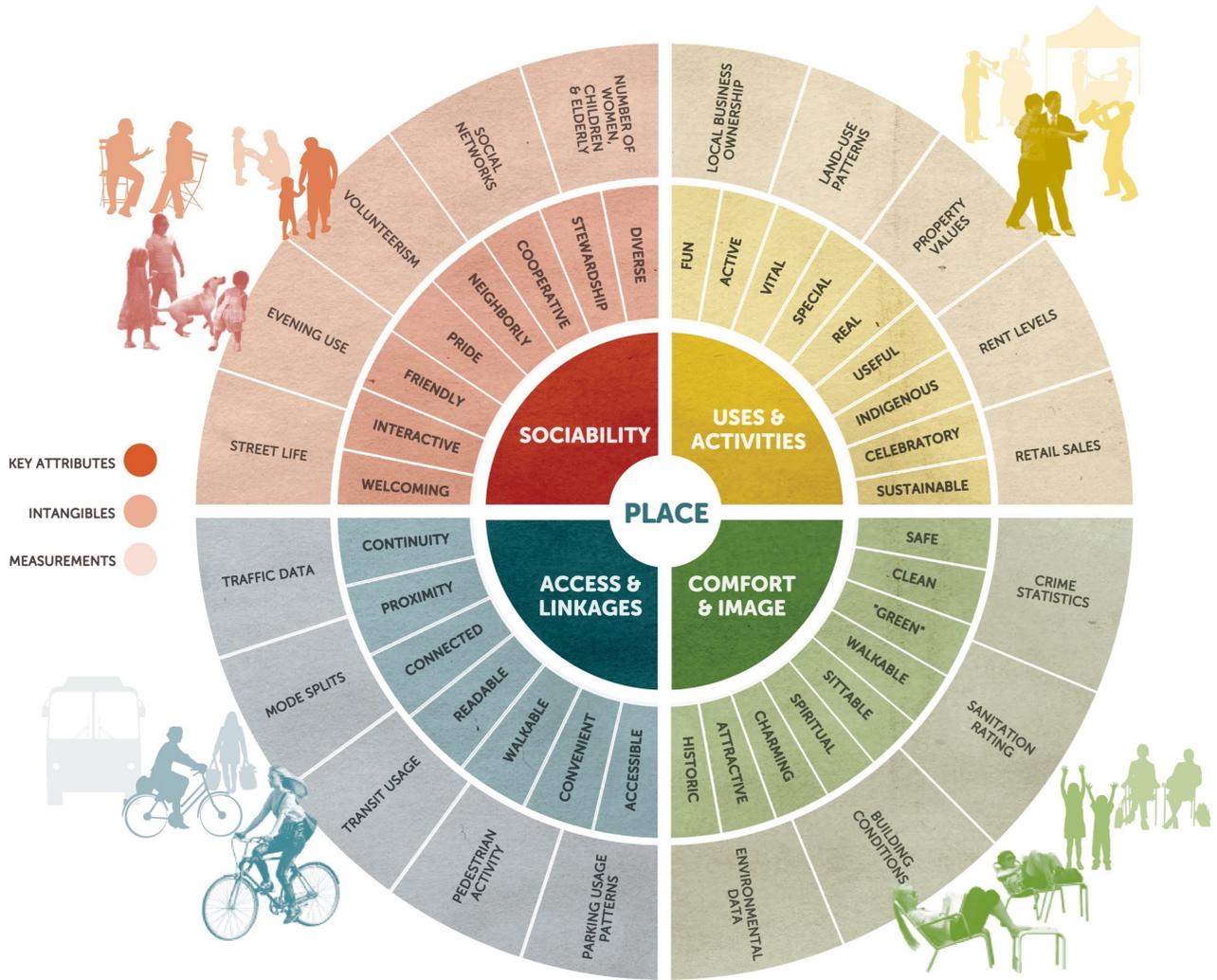
1.1 DOWNTOWN MASTER PLAN OVERVIEW

Goals of the Downtown Master Plan

- Create framework for great places & spaces
- Make downtown family-friendly
- Provide spaces for teens and older adults
- Promote greater variety of land uses including grocery, retail and restaurants
- Expand sidewalks and add shading to support retailers and restaurants
- Align with Kyle's planned Vybe trail network
- Include public input in decision making
- Increased connectivity and linkages for pedestrians, cars, bicycles, and golf carts
- Establish Mary Kyle Hartson Park as the center of downtown
- Engage local students and teens in activities to beautify downtown
- Preserve current residents' way of life and enjoyment of property as long as they desire
- Improve pedestrian safety comfort
- Improve public health
- Increase residential living options including apartments and townhomes
- Consider current and future parking needs
- Create a unique downtown that modernizes Kyle's history and charm
- Increase sociability, uses & activities, access, comfort and image



"Old Meets New" Kyle Creamery Pop-up Gallery Concept



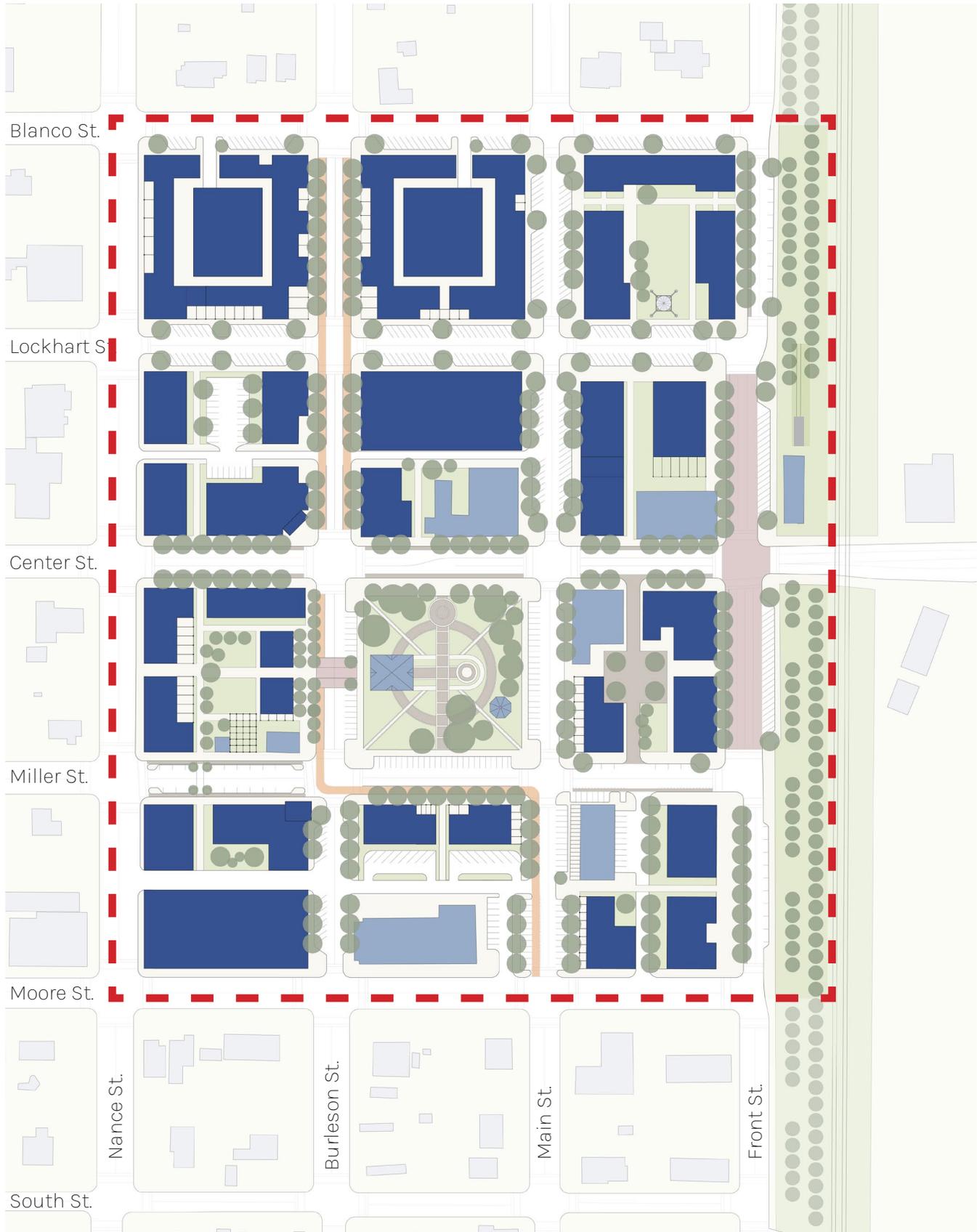
What Makes a Great Place? from *How to Turn A Place Around* by Project for Public Spaces

Existing Downtown



Disclaimer: The master plan is conceptual and used to illustrate good planning strategies. It is not a legally binding document. Existing residents, tenants, and property owners are under no obligation to take action at any time.

Long Range Downtown Master Plan



--- Downtown Master Plan Boundary

Test Projects

Test projects are relatively inexpensive ways to draw people and attention to the focus area. Multiple opportunities have been identified for downtown Kyle.

#1 Alleyway activation

The alley across from City Hall has the appropriate scale and location to be activated with string lights, wall mural, tables and chairs. Uses might include: public park for general use, pop up dining space with adjacent food truck or more. Recommendation to begin discussions with Arts Commission.



Center Street Alleyway Condition - Before



Center Street Alleyway Condition - After

#2 Kyle Creamery

The Kyle Creamery near Mary Kyle Hartson Park is a great opportunity to enhance Kyle's charm. A low-cost option would be to simply illuminate the structure. A more intensive option would be to make the building safe for occupancy and use it as a pop-up event space. Pop-up uses could include an ice-cream shop, a Kyle-focused gift shop, or a local art gallery.



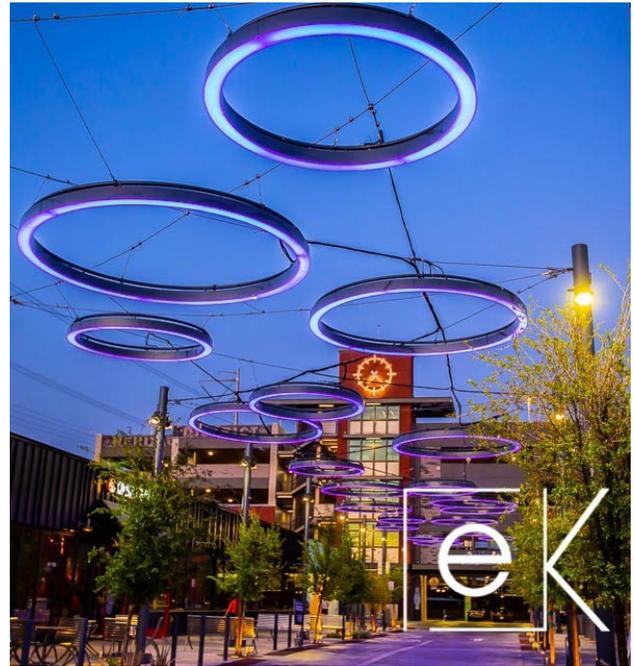
Kyle Creamery - Before



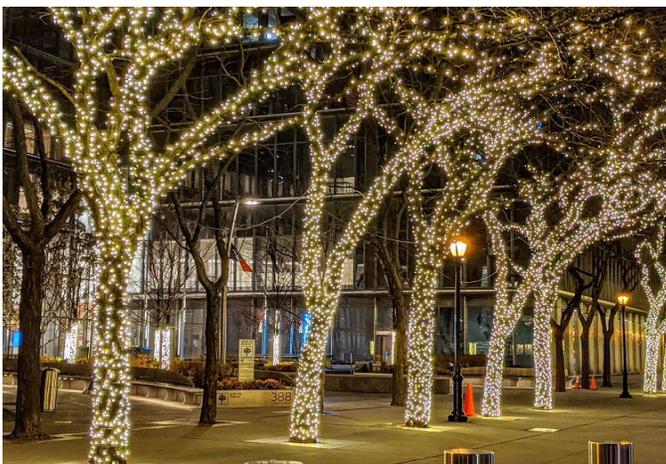
Kyle Creamery - After

#3 Illumination

Downtown could be activated year round with illumination. Lighting the water tower with color changing LEDs, adding suspended lights above Front Street, and lighting street trees along Burleson would help beautify downtown. Lights could be chosen by and installed under the guidance of local artists with assistance from the Arts Commission.



Front Street Lighting Concept



Burleson "Vybe Trail" Tree Lighting



Illuminated water tower

#4 Student & Local Artists

Existing city infrastructure can become a potential canvas for local students and artists. Examples of public infrastructure include: traffic control boxes, crosswalks, walls, light poles, public spaces. Students could submit ideas in the form of a competition and the public could vote on their favorite designs. Installations are temporary and new competitions could be held each year.



Painted traffic control box, Pamela Summers



Mural wall, Artist Daas



Temporary Crosswalk Installation

1.2 EXISTING ARCHITECTURE

The architecture found in Downtown Kyle speaks to the quiet charm of the City. Some of the City's oldest buildings, such as the Kyle Creamery and the Old D.A. Young building, have stood for almost a century through the use of ashlar masonry cut from local limestone. The wooden Train Depot, built in 1917, was relocated and then maintained for the last century through preservation efforts. A multi-year restoration of the building was completed in September 2016, and now, "the building houses a museum with displays showing the history of the Kyle Station..." (Kyle Train Depot).

At the heart of Mary Kyle Hartson park stands one of the "...most historically significant buildings in the City of Kyle...the historic 1912 Kyle City Hall. Constructed in the Mission Revival style by architect R.L. Thomas of Austin, Texas, the building remains at the heart of the central community park and is a defining landmark to downtown Kyle. The City Hall has undergone many alterations through the years, hiding or eliminating a significant amount of the building's historic character," (Kyle City Hall). It was recently restored by Architexas for community use. Through this process, the exterior brick masonry and stucco was restored to their original style, and several repairs were made for accessibility and longevity of the building.

Several of the civic buildings, such as the Kyle City Hall and the Kyle Public Library, feature a balance of Texas Limestone and red brick with a renaissance influence seen in the precise repetition of pairs of arched windows along the upper floor.

The buildings along north Center Street are characterized by single-story elevations, rustic canopies shading the sidewalk, and older brick and stucco facades. Renovations to some of these facades used thin split face masonry that ties in well with the existing architecture.

Newer buildings downtown have done a good job of honoring the local character. 105 S. Main Street, for example, uses a warm tan and red brick blend that matches nicely with the local palette and its two stories are in scale with surrounding structures. The Howry Building a little further down Main Street features maroon brick and is also two-floors tall, and both of these buildings incorporate overhangs for pedestrian comfort.

Overall, the architecture of Kyle is characterized by the use of masonry, usually brick or cut limestone, stucco, and wooden accent materials. The scale of these buildings tends to be on the smaller side, giving many of these buildings an approachable and friendly quality, an apt reflection of their city.



Restored (Former) Kyle City Hall; presently the Krug Activity Center



The Kyle Railroad Depot and Heritage Center



The Texas Pie Company on Center Street



Kyle Public Library, built in 2012



The Old Kyle Creamery



105 South Main Street, built in the 2010's



Live Oak Lodge, built in 1911



The Howry Building, built in 2016



Buildings along north Center Street



Kyle City Hall, built in 2005

1.3 EXISTING PARKING & TRAFFIC CONDITIONS

Current Traffic Conditions

Downtown Kyle currently sees on average 12,564 vehicles pass through its core daily (measured at the intersection of Burleson St. and Center St.), while approximately 15,270 vehicles pass through the intersection of Front St. and Center St. daily. This change in number may indicate that many residents exit through side streets away from Downtown Kyle rather than passing through Downtown.

Residents also indicated this trend in their responses to the Downtown Master Plan Online Questionnaire, where ~25% of respondents described the traffic downtown as “always bad”, while almost 60% said that traffic is “only bad during special events or rush hour”. However, several residents only use downtown streets during their rush hour commute or during special events, not during other times.

Current Parking Conditions

As of July 2022, there are 376 parking spaces available to the public in Downtown Kyle. Roughly 2/3 of those are street parking spaces, while the remaining 1/3 are spaces in surface parking lots. Certain streets, such as Main Street and Center Street, have had parking and infrastructural improvements, as well as the perimeter of the newly-renovated Mary Kyle Hartson Park. Residents have greatly appreciated these improvements, and they hope to see more of them. Over 40% of respondents to the Downtown Master Plan Online Questionnaire requested improvements in parking overall downtown.

Annual Average Daily Traffic (2021) through Downtown Kyle

**measured by TxDOT at the intersection of Center St. & Front St.*

15,270 Vehicles



Parking Conditions along Center St, looking westward

Current Parking Statistics (# of Spaces)

Street Name	Street Parking	Parking Lot
Center St	45	12
Front St	29	48
Main St	101	10
Burleson St	32	0
W Lockhart St	30	40
W Miller St	20	9
TOTAL	257	119

GRAND TOTAL: 376 parking spaces

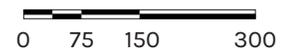
Existing Parking Conditions Map



--- Downtown Master Plan Boundary

— Street Parking

■ Parking Lot



1.4 EXISTING PEDESTRIAN CONDITIONS

Over the past decade, Downtown Kyle has received sidewalk and paving improvements that contribute to its walkability. As seen in the reference photo to the right, Center Street intersections are now marked with terracotta colored stamped concrete and the sidewalks have been expanded at street corners and specific parts for street trees. These additions have improved the Center Street pedestrian experience and establishes a good for surrounding streets to follow.

Accessibility

The “Current Pedestrian Conditions” Map to the right highlights existing sidewalks, as well as points of accessibility. “Ideal Accessibility” means that there is a ramp on both sides of the corner that is wheelchair accessible by sidewalk. Points where “Accessibility could be improved” may have a ramp on only one side of the corner, or the sidewalk and/or ramp isn’t well-maintained and would be difficult for a wheelchair user to navigate. “Inaccessible” points are characterized by a lack of a ramp, the ramp being completely disconnected from the sidewalk, or no sidewalk and/or ramp present.

Additionally, for visual clarity, existing sidewalks have been outlined to also bring attention to the lack of sidewalks along the majority of streets (excluding Burleson and Center Street). There is a lack of marked crosswalk intersections outside of the city blocks along Center Street, as well as several unmarked crossings where driveways or parking lots interrupt the sidewalk.

Sun and Shade Balance

Currently, several of Downtown Kyle’s sidewalks lack any shade. Center Street and portions of Front Street have great examples of shading with canopies and/or overhangs extending from their building’s facades, or through the use of street trees. As downtown continues to develop, the increase in paved surface area and buildings will naturally contribute to a “heat island” effect due to a lack of vegetation to serve as a “heat sink”.



Example of Ideal Accessibility conditions; textured wheelchair ramps on each side of the street corner connected to a well-maintained sidewalk and a clear crosswalk



Example of Inaccessible conditions; lack of textured wheelchair ramps on each side of the street corner, and sidewalk is disconnected from street crossing, with no sidewalk on the other side of the road; lack of a clear crosswalk

Existing Pedestrian Conditions Map



--- Downtown Master Plan Boundary

— Existing Sidewalks

//// High-risk Rush Hour Pedestrian Crossing

● Ideal Accessibility

● Accessibility could be improved

● Inaccessible

1.5 PUBLIC ENGAGEMENT

BGK was hired by the City of Kyle in September 2021 to draft a proposal for a new Downtown Master Plan encapsulating 12 blocks of Downtown Kyle. To ensure this proposal reflects of the community's needs, BGK studied existing plans of Downtown and did multiple site visits, as well as engaged in several meetings with local stakeholders and city representatives. Additionally, an online questionnaire created by BGK and the City of Kyle was released to get feedback from Kyle residents on some ideas being considered for the proposal to be able to make adjustments accordingly. Listed below are some of the major events and findings in this process.

Downtown Master Plan Expert Workshop

On March 22nd, 2022, BGK met with several departments of the City of Kyle government to review the Downtown Master Plan proposal. Those in attendance included: directors and representatives from the Planning Department, the Economic Development Department, Parks and Recreation, and the Communications Department, as well as the City Engineer and the City Manager. This meeting allowed for BGK to gain insight into the city's needs and concerns, and gave clearer direction in what materials BGK would need to provide the city to help with its implementation in the coming years.



Earth Day Outreach Event

Earth Day Event Outreach and Questionnaire Launch

On April 22, 2022, BGK attended the Parks and Rec department's Earth Day event to connect with the residents of Kyle in-person. Three large boards were displayed with information on the history of Kyle and its architecture, as well as a visualization board of potential spaces that could be part of future Downtown Kyle. The third board featured a link to the online questionnaire and information about the planning process. These boards allowed BGK to gauge residents feelings towards downtown currently and what they would like to see in the future.

Movie Night Outreach Event

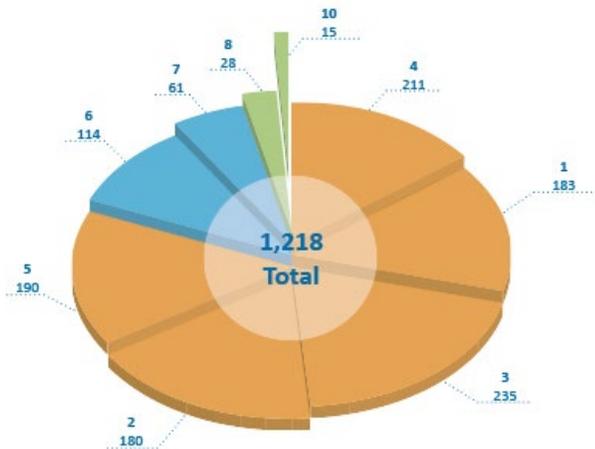
On May 27th, 2022, BGK attended the "Movies in the Park" event to connect with young residents of Kyle in person. Two large boards were displayed, on one board the question "What do you love about downtown Kyle?", and on the other board the question "What would you like to see in downtown Kyle?". Next to the boards laying on the table were 3 different color post-it notes. The different colors represent different ages of the participants. The green was for ages 0-5, yellow 6-11, and purple 12+. Children as well as parents were then asked to answer the questions on the boards using the post-it notes that corresponded with their ages. In doing this survey, BGK was able to see what young people as well as young families were looking to see in downtown Kyle.

1.6 QUESTIONNAIRE RESULTS

To hear from a greater number of Kyle residents, BGK and the City of Kyle worked together to create a 20-question online questionnaire that would be distributed to attendees at in-person events and online through the City of Kyle website and its social media. This questionnaire was comprised of questions that would provide insight into how residents feel about the current state of Downtown Kyle, as well as what they would like to see downtown in the future.

The questionnaire was made available on April 22nd, 2022, and closed June 8th, 2022 to give residents a large amount of time to respond. All questions that used a scale of 1-10 designated 1 as the lowest ranking, and 10 as the highest ranking. Feedback from respondents confirmed their understanding of the ranking system. 1259 responses were collected with a completion rate of 88%, meaning 88% of respondents completed the entire survey, and the remaining number primarily skipped open-ended questions while still providing answers to the majority of questions.

On a scale of 1 to 10, How vibrant is your Downtown?



Earth Day Outreach Event



Movie Night Outreach Event

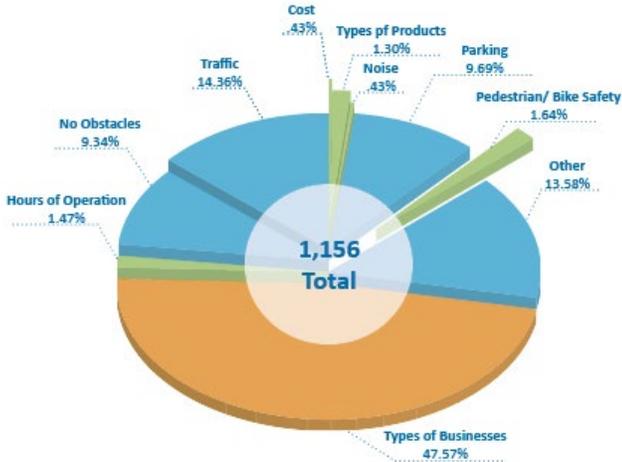


Movie Night Outreach Event

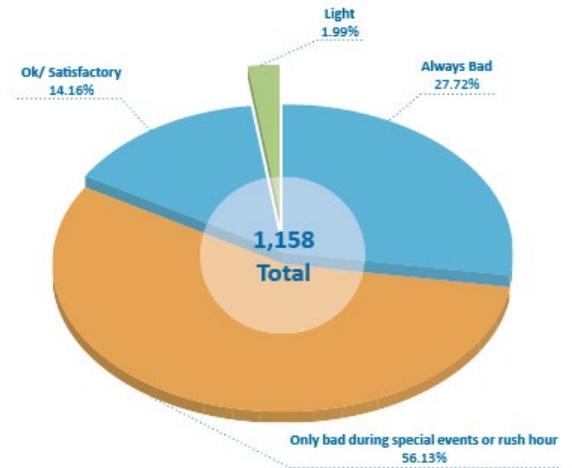


Movie Night Outreach Event

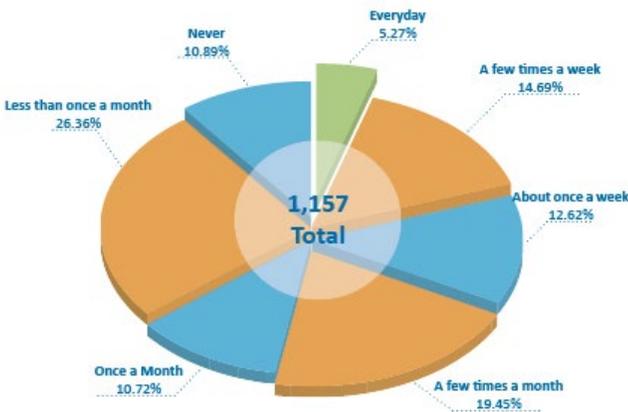
What is the Biggest Obstacle when visiting Downtown?



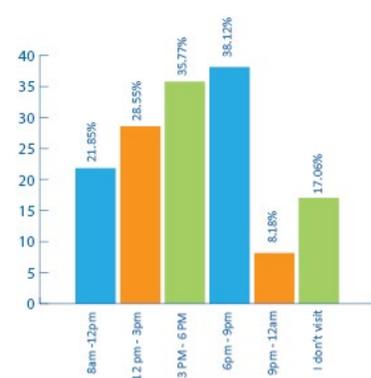
How would you describe the traffic downtown?



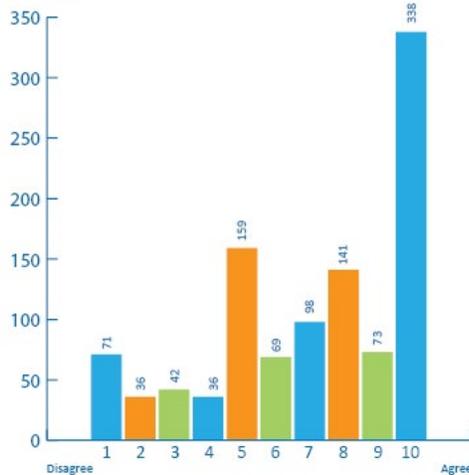
How often do you visit Downtown Kyle?



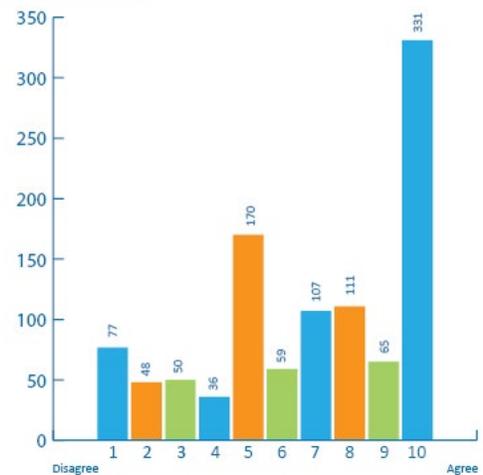
What time of day do you typically visit downtown?



I would like downtown would to be bigger.



I think downtown should have bigger sidewalks.



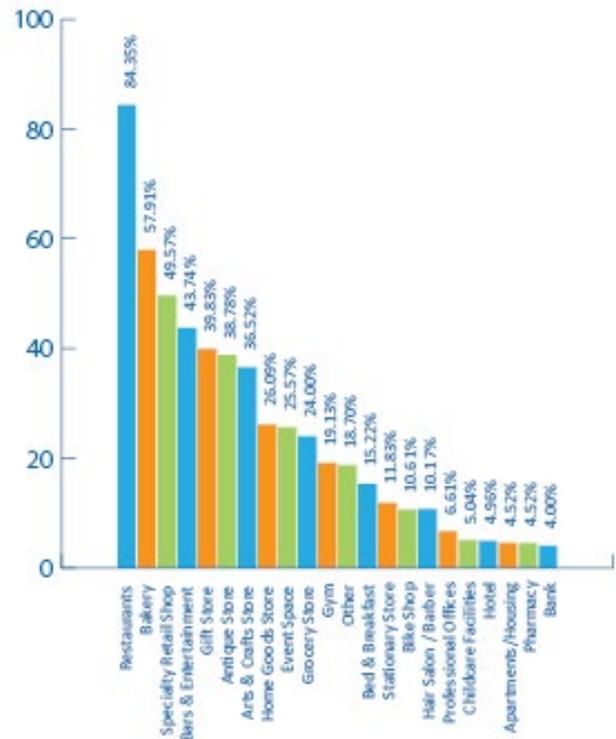
1.7 KEY TAKEAWAY FROM PUBLIC INPUT

The online questionnaire provided a great deal of insight into residents' feelings towards downtown Kyle and what they would like to see in the future. It also gave BGK and the City of Kyle a quantifiable sense of what kinds of businesses and spaces residents are looking forward to in the future, as well as the sort of infrastructural and spatial improvements that residents are hoping for. Lastly, concerns that had been conveyed by residents and city leadership in prior meetings regarding traffic and other issues downtown was further confirmed by the questionnaire results.

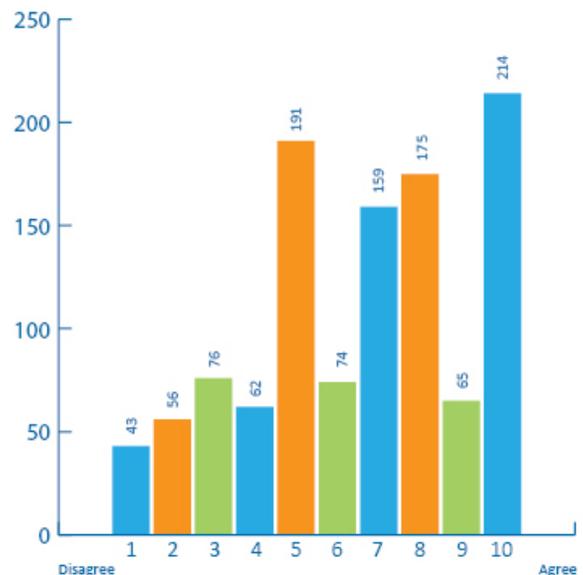
Overall, the major takeaways from the online questionnaire are that Kyle residents want:

- Restaurants, bakeries, specialty retail, entertainment venues, and gift/antique stores, among several other business types. Residents would like these businesses to be locally-owned and connect to the community.
- More family-friendly spaces and activities, especially spaces that are geared towards kids and teens. Water features such as a splash pad were specifically requested by several children.
- Larger sidewalks, more trees and landscaping, and an improved pedestrian experience overall.
- Larger Downtown proper, with a greater definition of character to make Downtown Kyle a "destination".
- Improved traffic conditions, especially for flow and amount of traffic during events or rush hour.

What would you like to see in downtown Kyle?



I'm proud to call Kyle home.



1.8 MASTER PLAN PHASES

The exact order and timing of privately owned development will evolve organically and cannot be predicted. This Downtown Master Plan is intended to be suggestive, not prescriptive.



Phase 1

- Test projects & pop-ups
- Up-size utility infrastructure
- Improve streets and sidewalks
- 104 S. Burseson Building
- Relocate VFW and create linear park

Phase 2

- Finalize train “quiet zone”
- Front St. Food Hall
- China Buffet & Historic Center Street Block
- Maint St. / MKH Park Mixed-use
- 104 Burseson / 210 Center St. corner

Phase 3

- 108 / 110 W. Center St
- Grocery Store at 300 W Center St.
- Mixed-use facing MKH Park
- Hotel/restaurant facing MKH park
- Parking garage on Lockhart St.
- Renovated Kyle Police Dept. Building

Phase 4

- Townhomes
- Multifamily wrap building
- Live-work units along Nance St.
- Mixed-use near Front & Moore St.

Phase 5

- Additional townhomes
- Second multifamily wrap building
- New plaza under Kyle water tower
- Second parking garage

Phase 1

Phase 1 is the first phase in the Downtown Master Plan and projected to take 3-5 years to complete from the project’s start date. This phase will consist of infrastructure improvements to water, electrical, and other infrastructural systems within the North-South bounds of Blanco St. to W. Moore St., and the East-West bounds of N. Front St. to Nance St. These improvements will ensure that the needs of any future spaces and businesses downtown can be met.

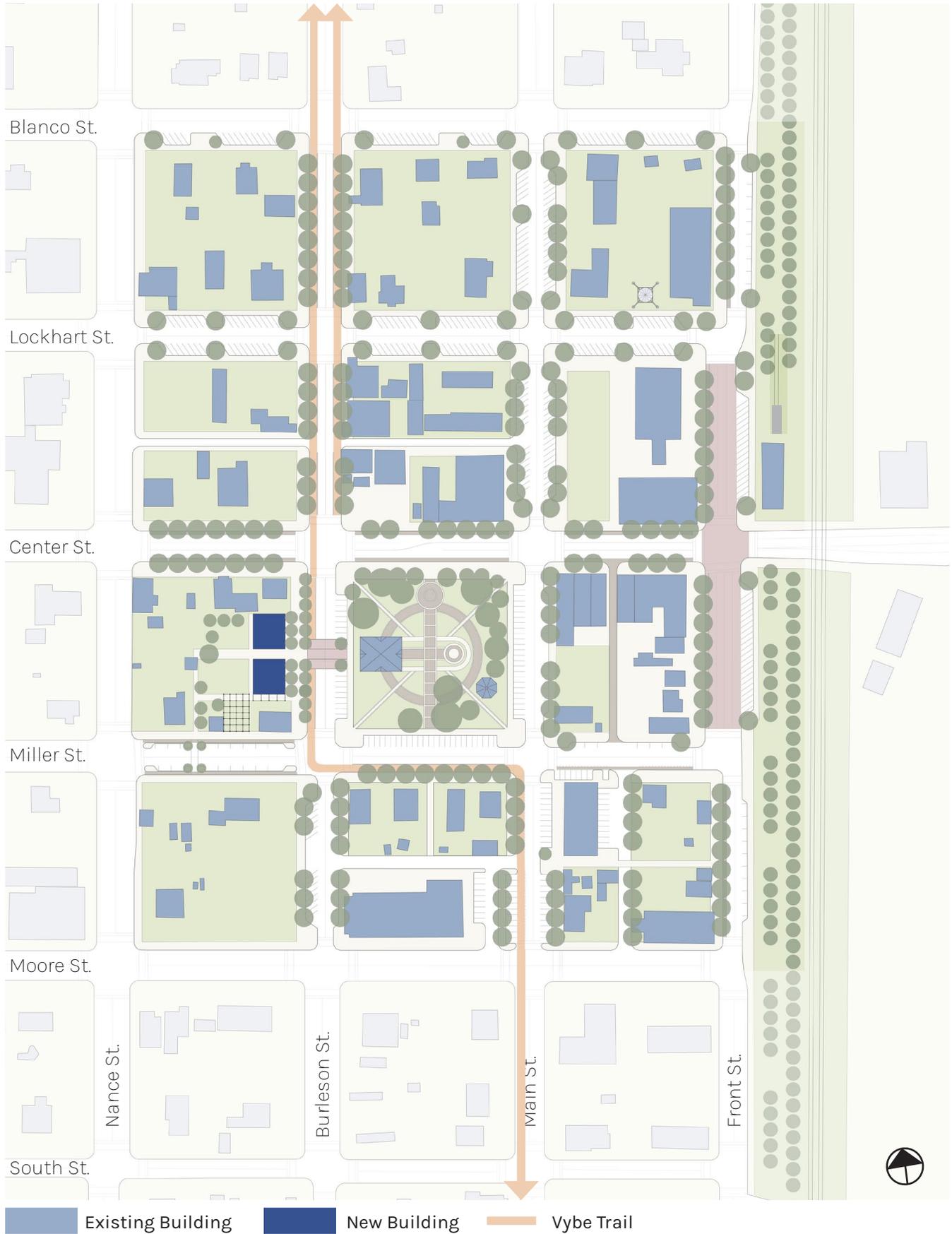
The majority of sidewalks downtown will be widened and feature new landscaping, street trees, and paving. This will be focused primarily on sidewalks that have not been renovated/rebuilt within the last 20 years.

A new civic building at 104 S. Burseson will be constructed catalyzing a new era for downtown Kyle with a focus on Mary Kyle Hartson Park.

Downtown streets will see updates to their paving and gradual lane structure adjustments to ease traffic congestion and beautify the general streetscape. Several streets will gain street parking, providing residents with additional parking options downtown.

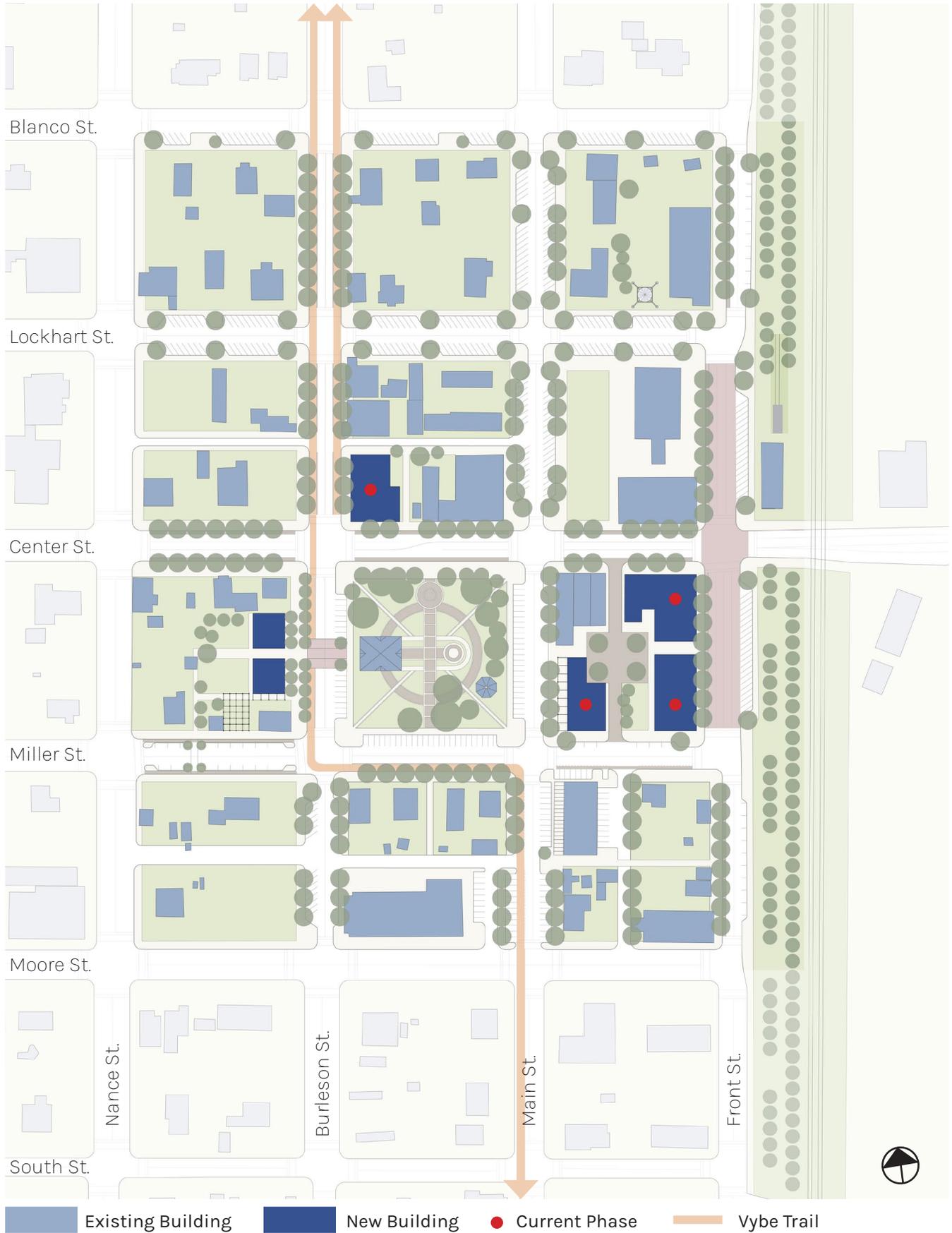
Disclaimer: The master plan is conceptual and used to illustrate good planning strategies. It is not a legally binding document. Existing residents, tenants, and property owners are under no obligation to take action at any time.

Phase 1



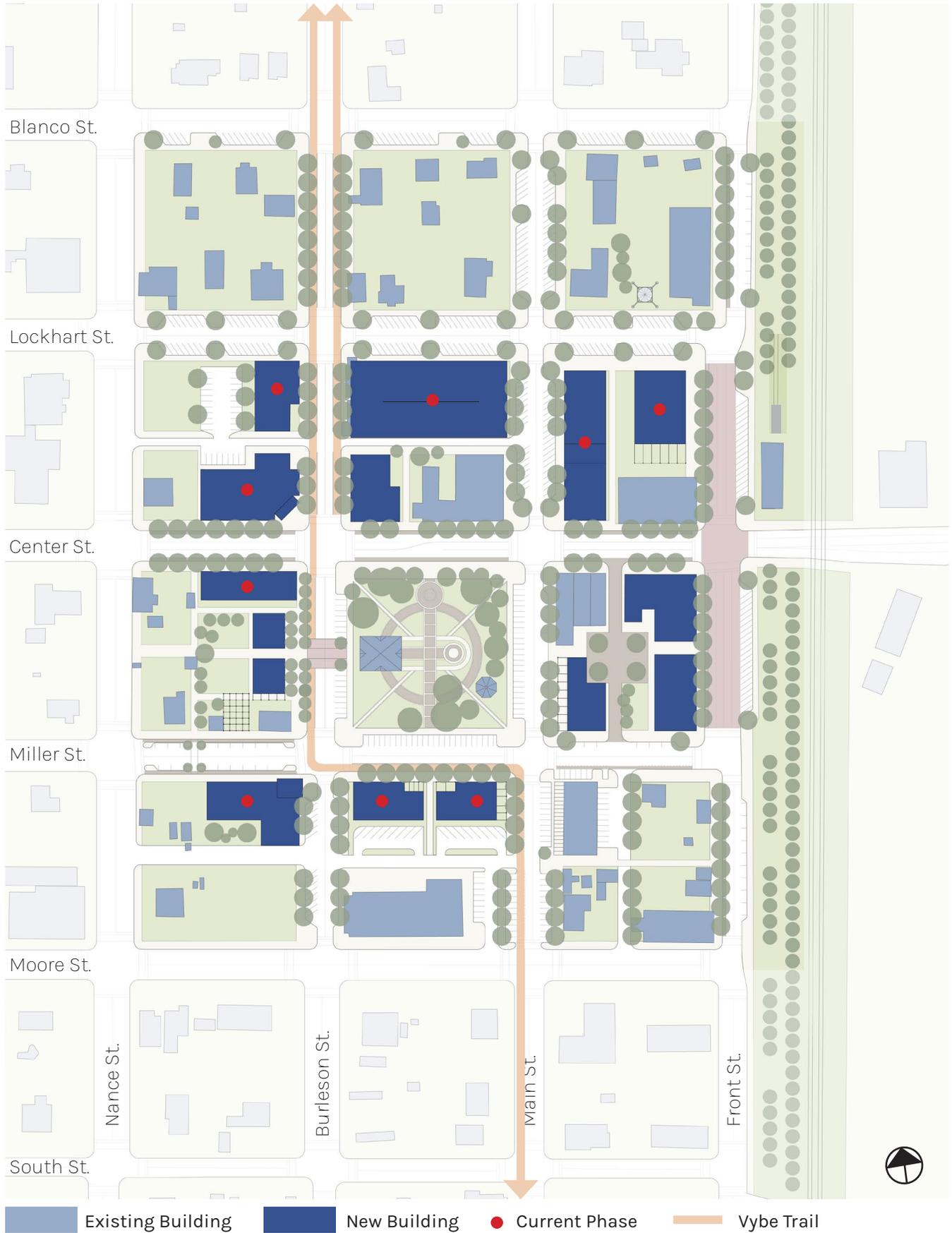
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Phase 2



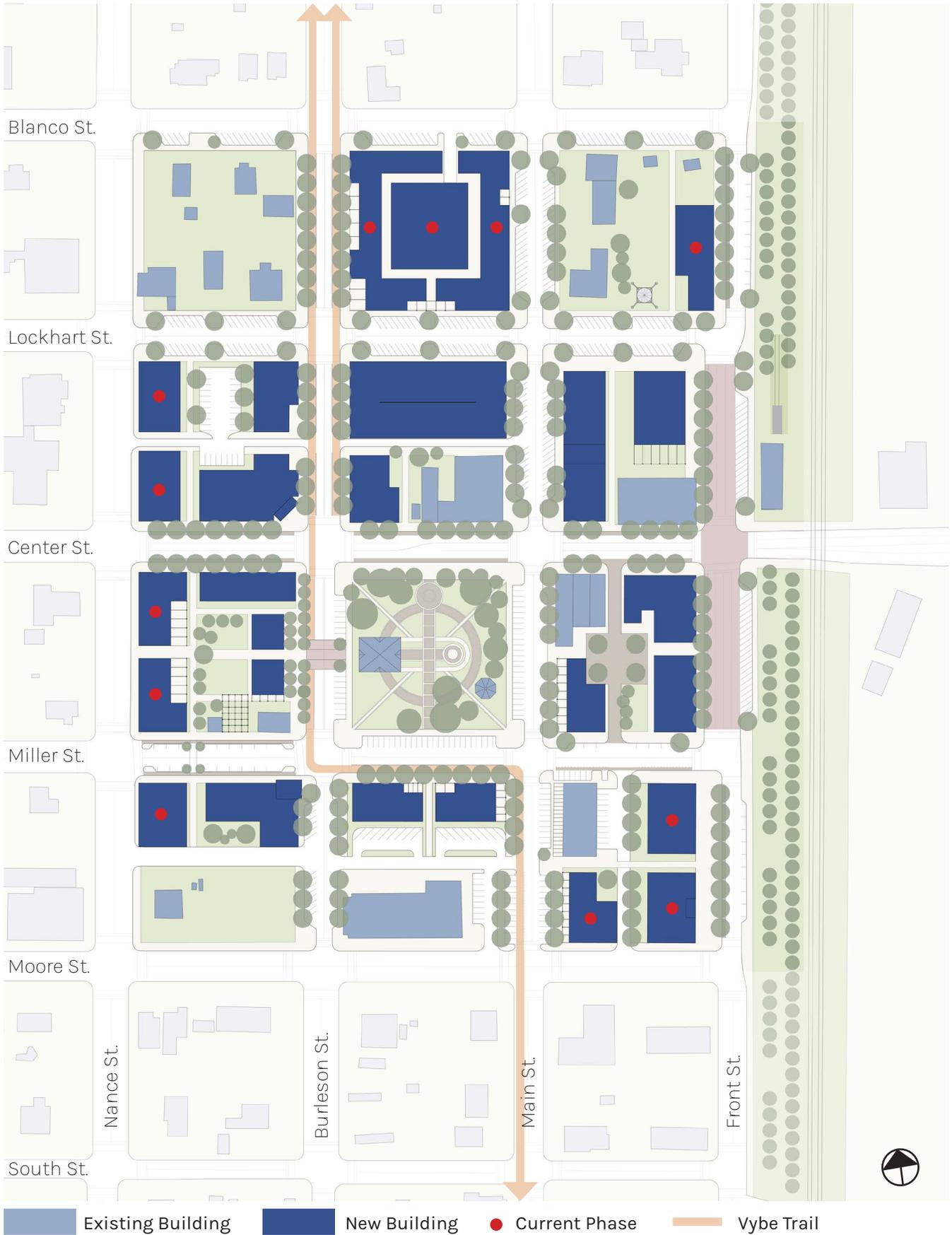
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Phase 3



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Phase 4



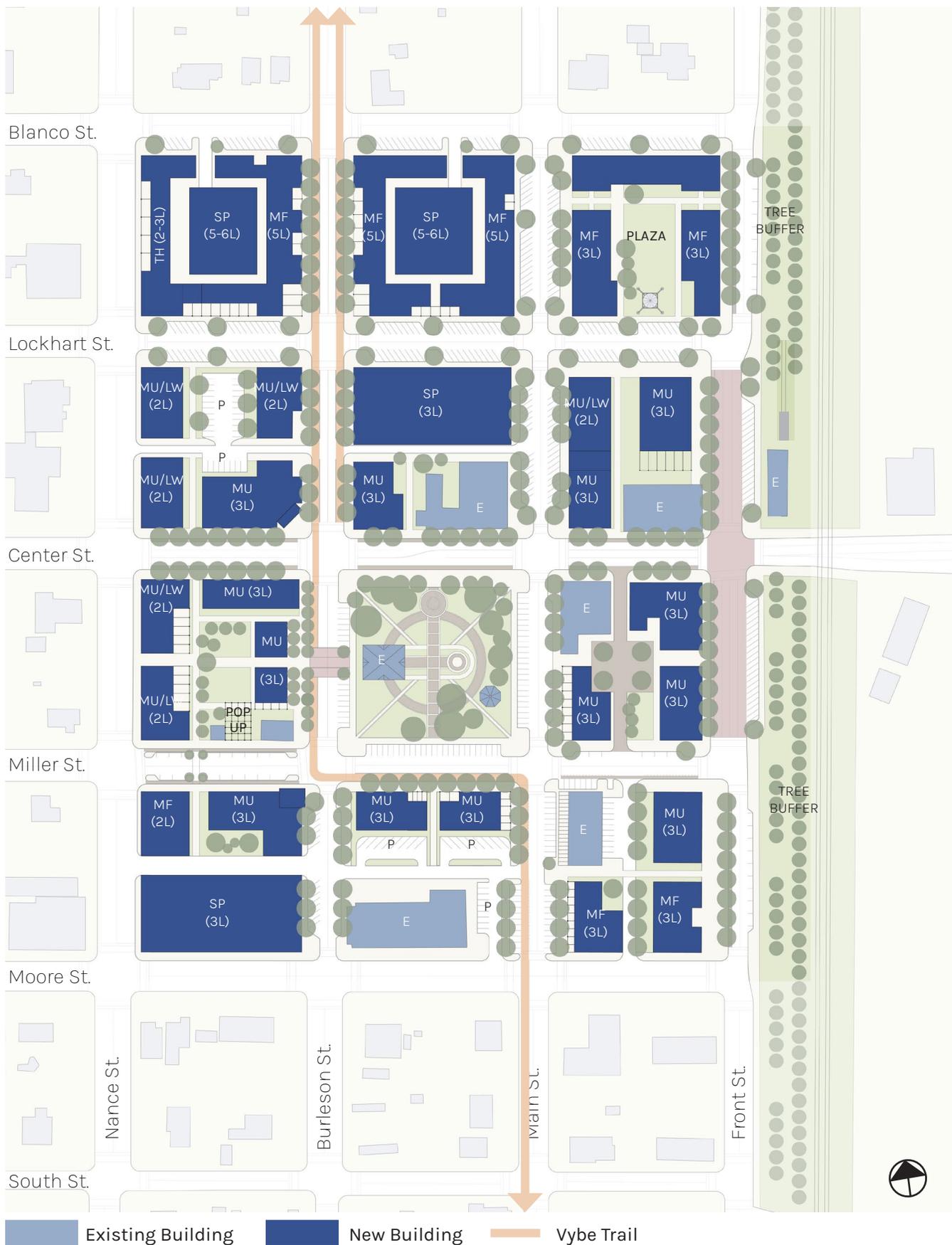
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Phase 5 “Long Range Plan”



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Suggested Use and # of Floors



Full Build-Out Metrics

- > 750 new residents
- 16 existing homes impacted
- > 400,000 sf of new commercial space
for retail / office / restaurants
- 15,000 sf grocery
- > 600 public garage parking spaces

TH	Townhomes
MU	Mixed-Use
LW	Live-Work
MF	Multifamily
SP	Structured Parking
P	Parking Lot
E	Existing Building
(3L)	3 Level Building
(2L)	2 Level Building

1.9 FRONTAGE ROAD FUTURE PHASE

Frontage Road (Future Phase)

The retail area between Highway 81 and the I-35 frontage road is beyond the scope of the downtown master plan. A conceptual planning sketch is provided however to illustrate that those design principles found in the organization of downtown should be extended into this area when appropriate.



Typical Existing Condition Along Frontage Road

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1.10 LOCATION OF USES

The city is eager to see a variety of uses populate Downtown Kyle, and highly encourages local businesses to locate in Downtown. Additionally, the city is interested in a hands-off approach that supports the community in deciding what places they want and need Downtown. For this reason, this section will be less prescriptive than other sections in this guidebook. With this being the case, there are only a few limitations listed below.

- **Residential Units** should be located along Blanco Street to integrate the fabric of Downtown Kyle with the existing neighborhood to the north. These can include **Multi-Family Residential Units and/or Single-Family Attached Residential Units (Townhomes)**. Only residential units should be located along this street.
- **Drive-through facilities** are prohibited within the 12 blocks defining the Downtown Master Plan to maintain a pedestrian-friendly downtown and reduce potential congestion created by drive-thru facilities

Outside of these limitations, the spaces Downtown are encouraged to flourish in a way that aligns with the community's wants and needs. In this vein, the Downtown Master Plan Online Questionnaire asked respondents what businesses and programs they would like to see downtown. Based on their feedback, suggested uses for some of these spaces that would support the community's vision include the following:



1) *Hotel on the Park* (Furman + Keil Architects)

A special two-story hotel with a quality cafe or restaurant will blend in with the Downtown scenery and invite out of town guests and locals to new amenities fronting Mary Kyle Hartson Park.

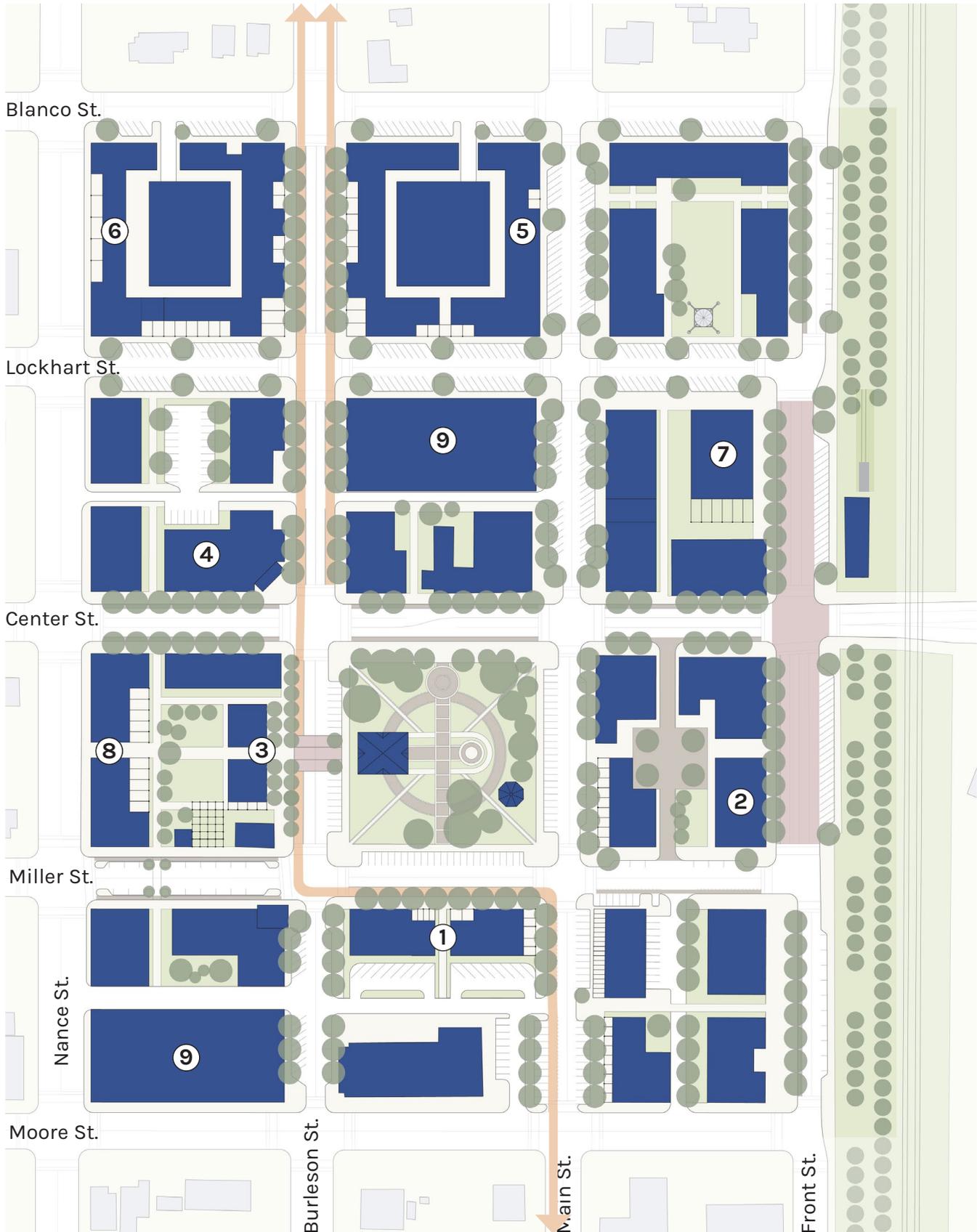


2) *Kyle Food Hall*

A food hall will offer several interior spaces for beloved local restaurants such as Lala's, Pizza Classics, Bobar, Hays City Chinese Cuisine, Cloud9 Treats, just to name a few. The community can come together under one roof for a fun dining experience, as well as for small-scale performances and local events in an airy, air-conditioned space.

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Recommendations for Particular Uses





3) 104 S. Burlison Building (BGK Architects)

A new mixed-use building across from the recently renovated Mary Kyle Hartson Park will help celebrate the charm and future of downtown Kyle. A restaurant / cafe will anchor the ground floor. The second floor will include flexible office space and the third floor will be home to a large open space for public and private events.



5) Parking Garage Wrap Apartments (Mithun)

Wrap apartment buildings are recommended for a few key areas of downtown. These will introduce parking and more residents to the area.



4) Fresh Foods Grocery (LRS Architects, Inc.)

A small grocery store of approximately 15,000 square feet, is suggested for the corner of Burlison and Center Street, across from Mary Kyle Hartson Park. A grocery with a variety of options and price points within walking distance of downtown residents will be key to a healthy city.



6) Townhomes (ADHOC Architects)

Townhomes add to the diversity of housing options that are key to a healthy city. Townhomes are a natural middle between single family homes and wrap buildings. Both owner-occupied and rental units within townhome buildings are acceptable.



7) *Mixed-use* (Wang Architects)

Mixed-use projects will bring more residential, retail, office, and restaurant spaces to the downtown area.



9) *Structured Parking* (Danze Blood Architects)

Two structured parking garages have been located to allow for all of downtown to be within a 5-7 minute walk from parking.



8) *Live-work* (Wang Architects)

Similar to a townhouse, a live work unit can include commercial retail or office space on the ground floor with living quarters above; owned or rented by the same occupant.

1.11 BUILDING SITING & PLACEMENT

Build-to-Zone

Except for buildings with ground-level residential or hotel uses and those mentioned below, the build-to-zone is a minimum of zero feet and a maximum of ten feet from the street-facing property line of the parcel. Along Center Street and for any parcels that maintain approval from the Planning and Zoning Commission, the build-to-zone can be extended to a maximum of 30 feet from the property to allow for outdoor space and seating for dining or other public activities.

New Buildings featuring Ground-Level Residential or Hotel Uses

The build-to-zone will be a minimum of five feet and a maximum of ten feet from the property line to establish a level of privacy between residents and the street.

Side and Rear Setbacks

The minimum setback distance required from the side of rear of buildings is zero feet, but can be no greater than thirty feet.

1.12 BUILDING HEIGHTS

General Building Height Requirements

Current zoning limits for Central Business 2 (CBD-2) stipulates a maximum building height of 45 feet. In accordance with this regulation, new buildings developed within downtown Kyle will fall within a range of one to three floors in height. Any plan that seeks to be greater than 45 feet in height needs to acquire approval from the planning and zoning commission and City Council, and is not allowed to exceed a height of 60 feet.

Non-Residential Ground Level Units

Ground floor-to-floor height will be 15 feet or greater. The finished elevation of the non-residential ground floor must be no more than 12 inches above the sidewalk finish level, but it is strongly encouraged that the ground floor be flush with the sidewalk to maximize accessibility of the space from the exterior.

Residential or Hotel Ground Floor Units

The finished elevation of the floor for residential or hotel ground floor units must be at least 18 inches above the finish level of the sidewalk, and may be no more than 60 inches above the finish level of the sidewalk.

Minimum Heights

Lots facing Mary Kyle Hartson Park - 2 levels
Lots along Center St. - 3 levels
Lots along Front St. - 3 levels
Lots along Burleson St. - 2 levels

1.13 FRONTAGE & TREATMENT REQUIREMENTS

Ground-Level Residential Treatment

Along streets that do not have street trees/natural shade available, such as South Front Street, shade features are required. These can include awnings, canopies or another shade feature that is appropriate to the context.

Roof Treatment

Buildings may make use of flat or sloping roofs, however, sloping roofs are encouraged/permitted on buildings three floors or less. Sloped roofs may be constructed with standing seam metal roofs, concrete/clay tile, or other complementary materials deemed by City Architect. Sloped roofs shall be designed with generous overhangs, exposed beams, decorative brackets of materials which are complimentary.

Retail Signage

A comprehensive coordinated signage program shall be prepared and submitted to the City Architect/Planning and Zoning for their review and approval.

Design Standards to include:

- Simplified allowable types, colors, sizes of retail signage based on location
- Establish a hierarchy of signage regulations, with the most stringent requirements for signage around Mary Kyle Hartson Park. More flexibility granted at CBD, Kyle Suburbs, and most flexibility at the I-35 Corridor Area:
- See appendix



1.14 HOURS OF OPERATION

Hours of Operation

Downtown businesses are required to remain open a minimum of 10 hours between 7am-11pm for businesses with more than 2 employees.

1.15 PEDESTRIAN EXPERIENCE

The pedestrian experience is integral to the experience of any city's downtown, and Kyle is no exception. Two-thirds of respondents to the Downtown Master Plan Online Questionnaire rated 5 or above (out of 10) to the statement, "I think Downtown should have bigger sidewalks". Additionally, several residents during in-person outreach events specified that they want improved walkability downtown and for it to be less car-reliant.

For these reasons, the following strategies address these needs in the Downtown Master Plan:

- Sidewalks have been designated along the perimeter of all 12 blocks of the Downtown Master Plan to maximize walkability and accessibility.
 - All proposed sidewalks are 9 feet wide or greater.
 - Increased width of sidewalk allows for businesses and building users to utilize their outdoor frontage space for gathering, dining, or advertising their goods.
 - Street Trees are proposed for over 50% of the street frontages downtown, and general landscaping is proposed for the majority of sidewalks.
 - Ample shading from street trees and awnings included to keep pedestrians and sidewalk surface temperatures cool
 - Sidewalks large enough to accommodate clusters of benches and street furnishings.
- 12 foot wide lanes for bike and cart use along Burleson Street celebrate its intersection with The Vybe, Kyle's planned trail network. Bike lanes are maintained along The Vybe's path of travel through downtown for a continuous experience.
 - Protected bike lanes through the use of raised medians and thoughtful parking planning.

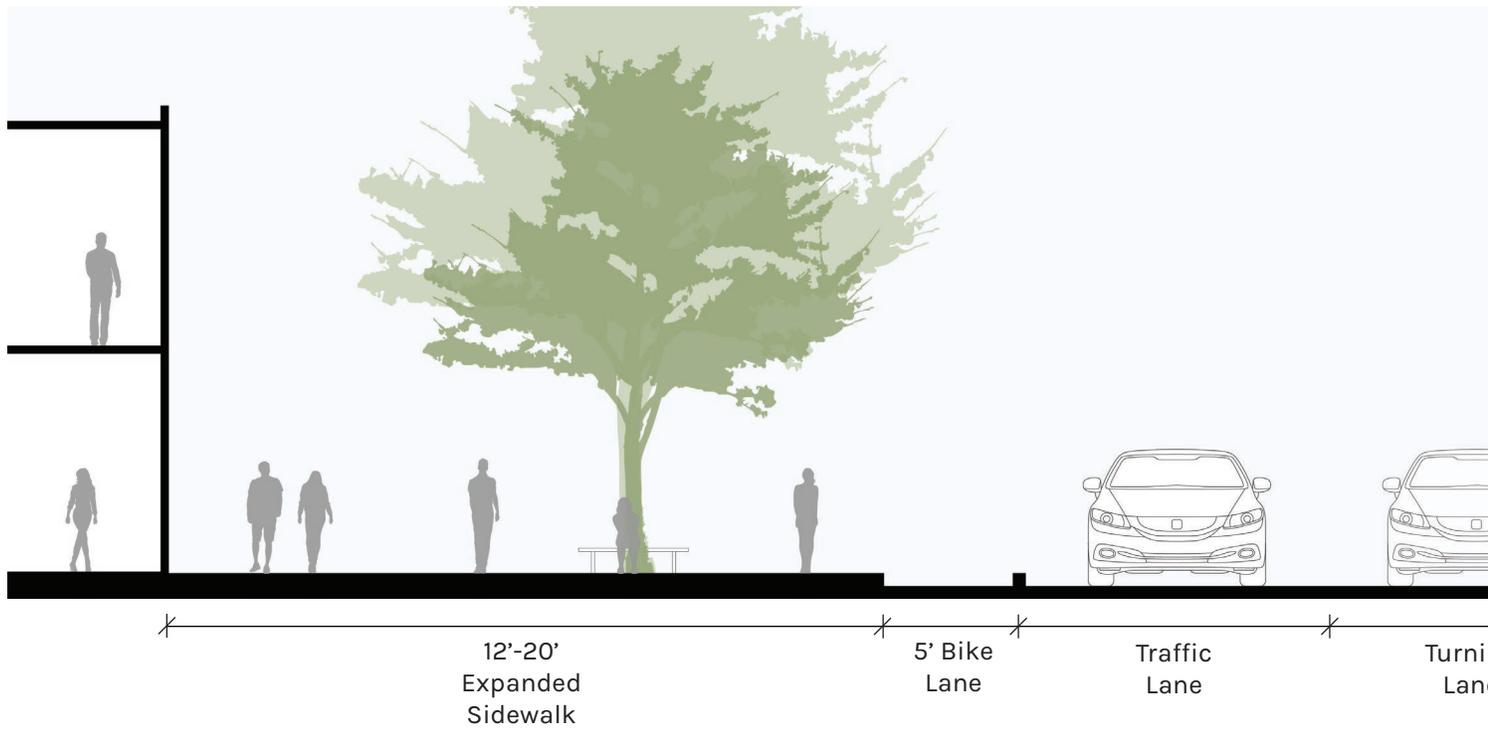


Existing city sidewalk



Widened sidewalks, public spaces, and street trees (BGK Architects)

Front Street - Cross Section



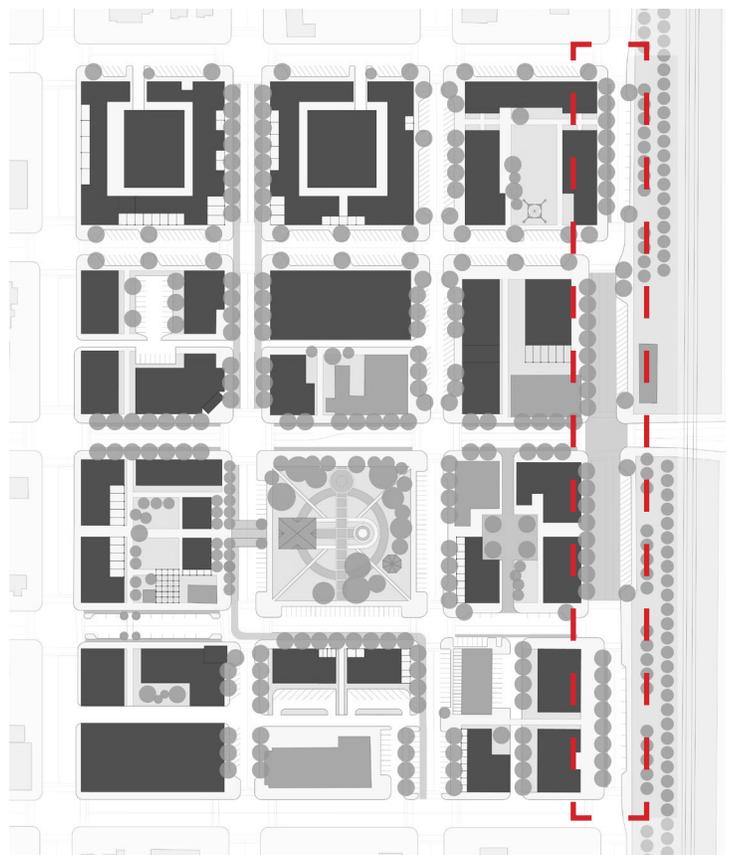


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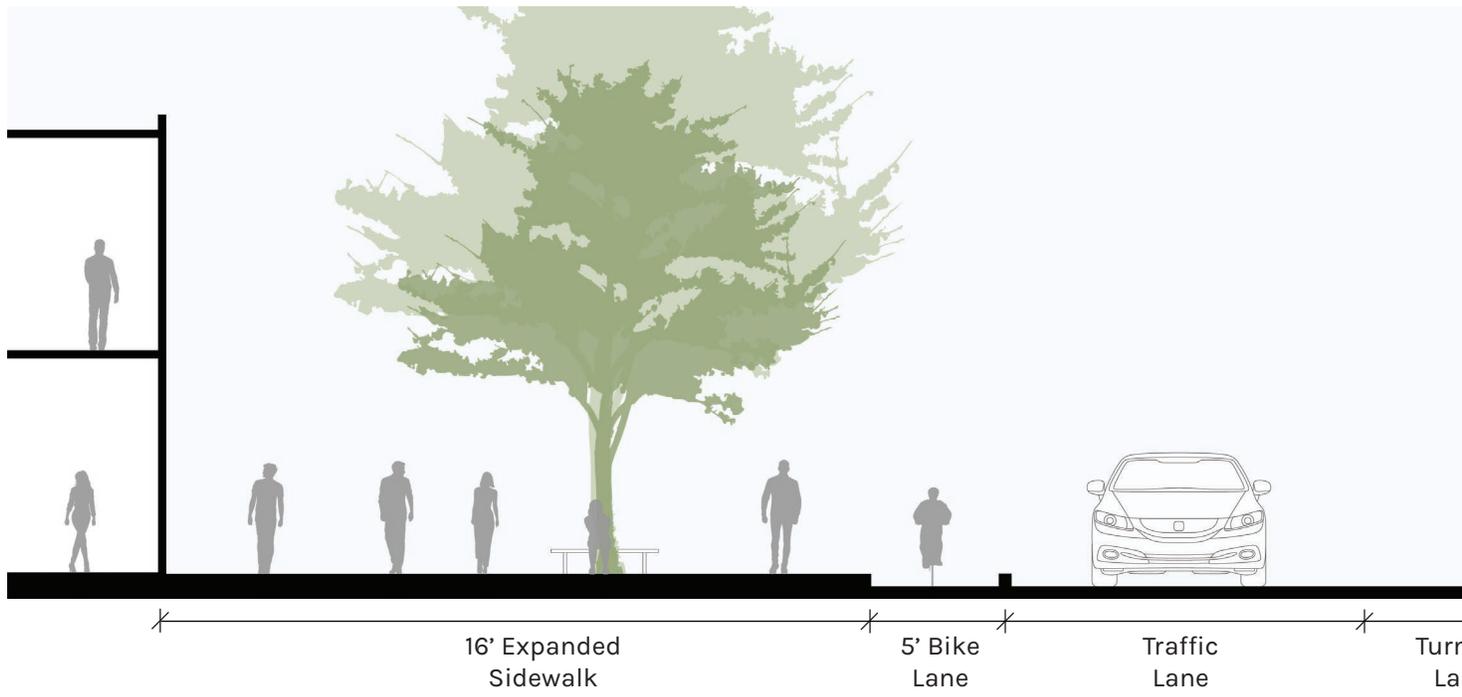
Traffic
Lane

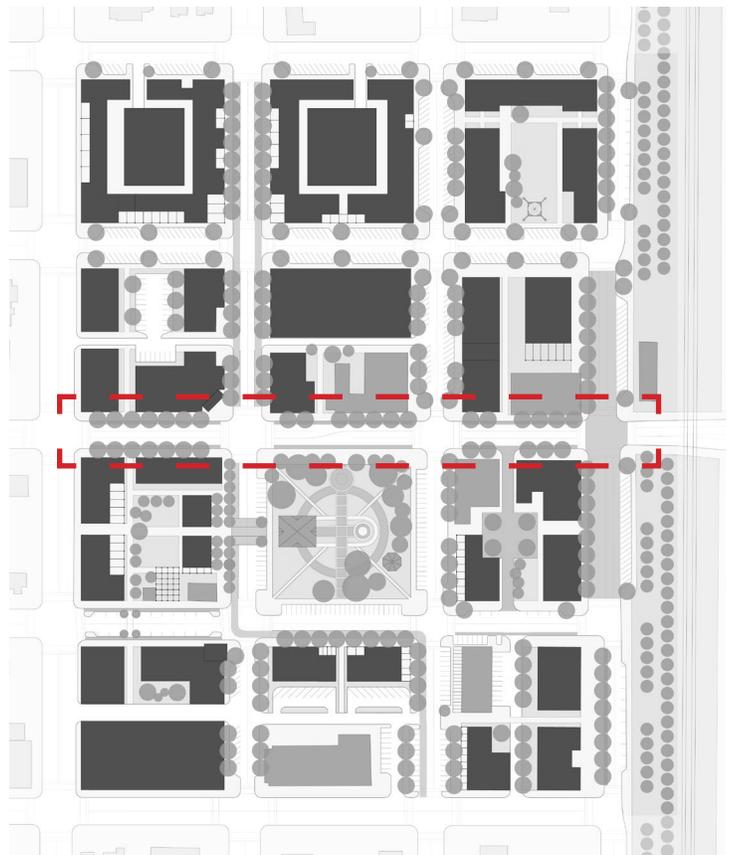
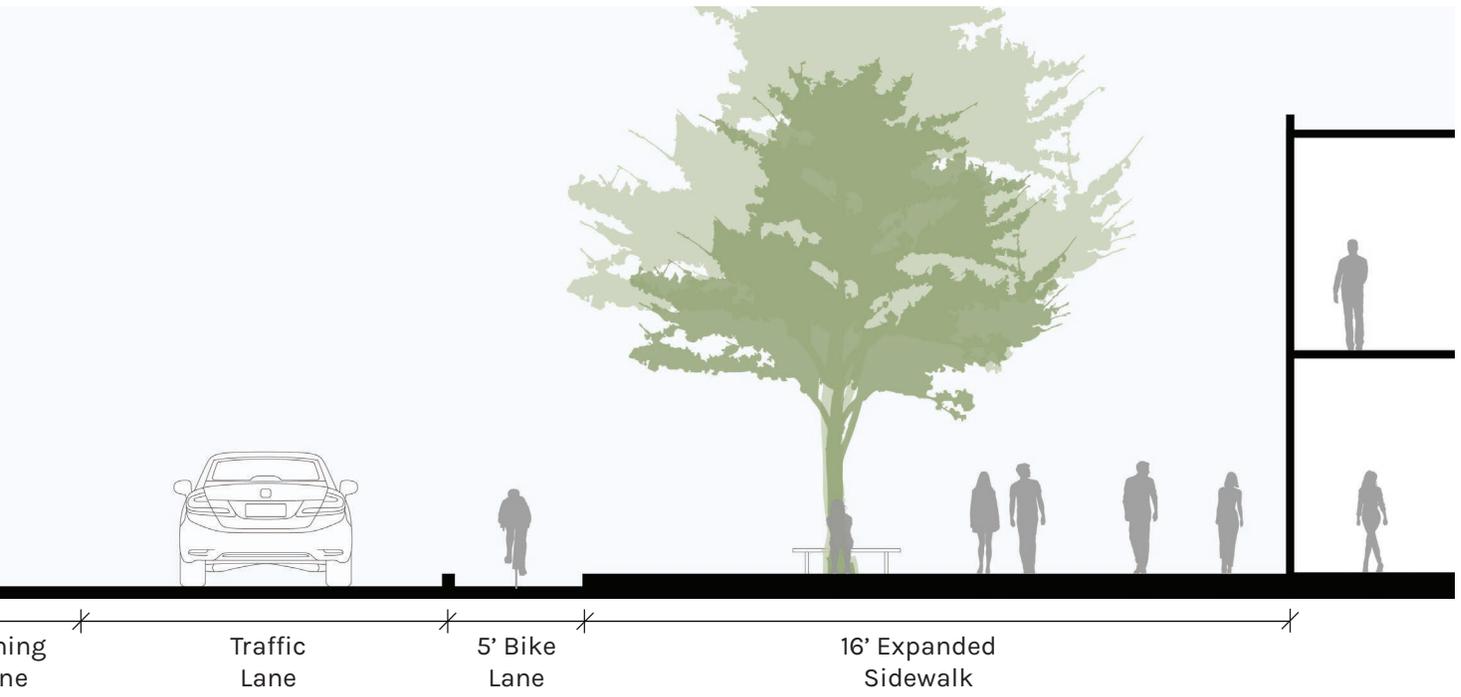
19' Angled
Parking

Expanded
Sidewalk

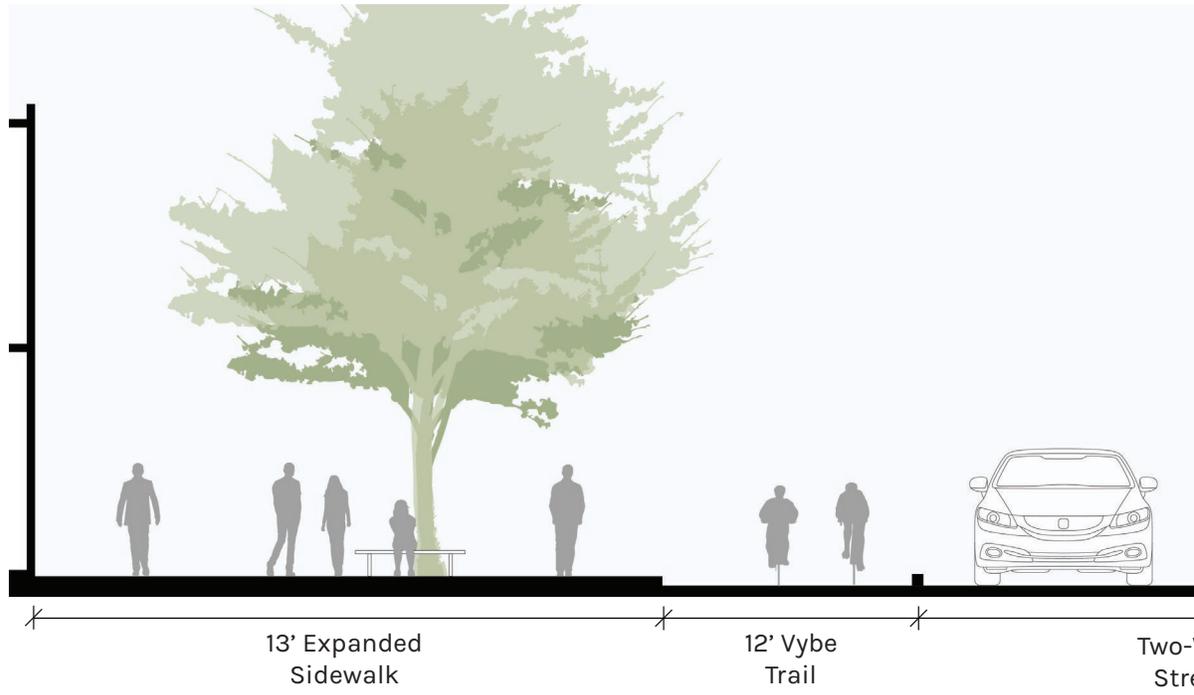


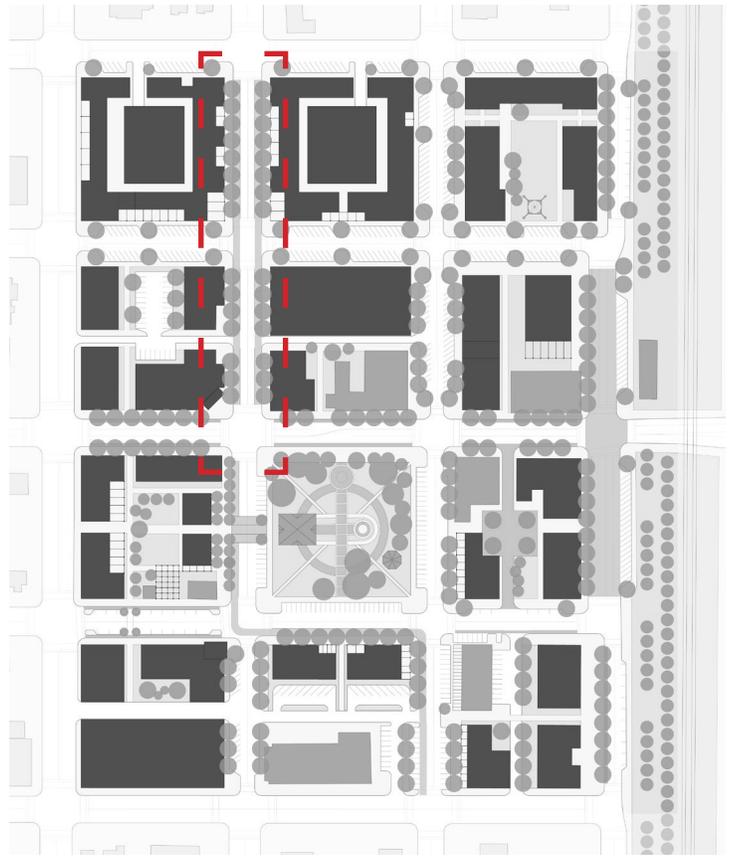
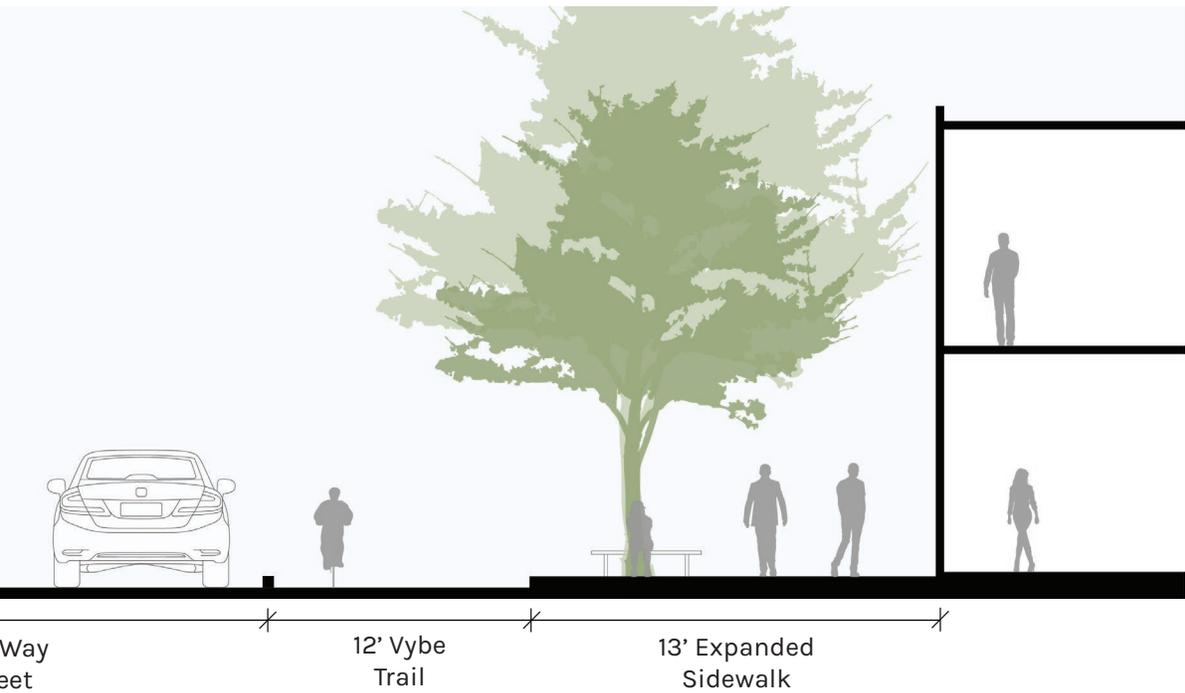
Center Street - Cross Section



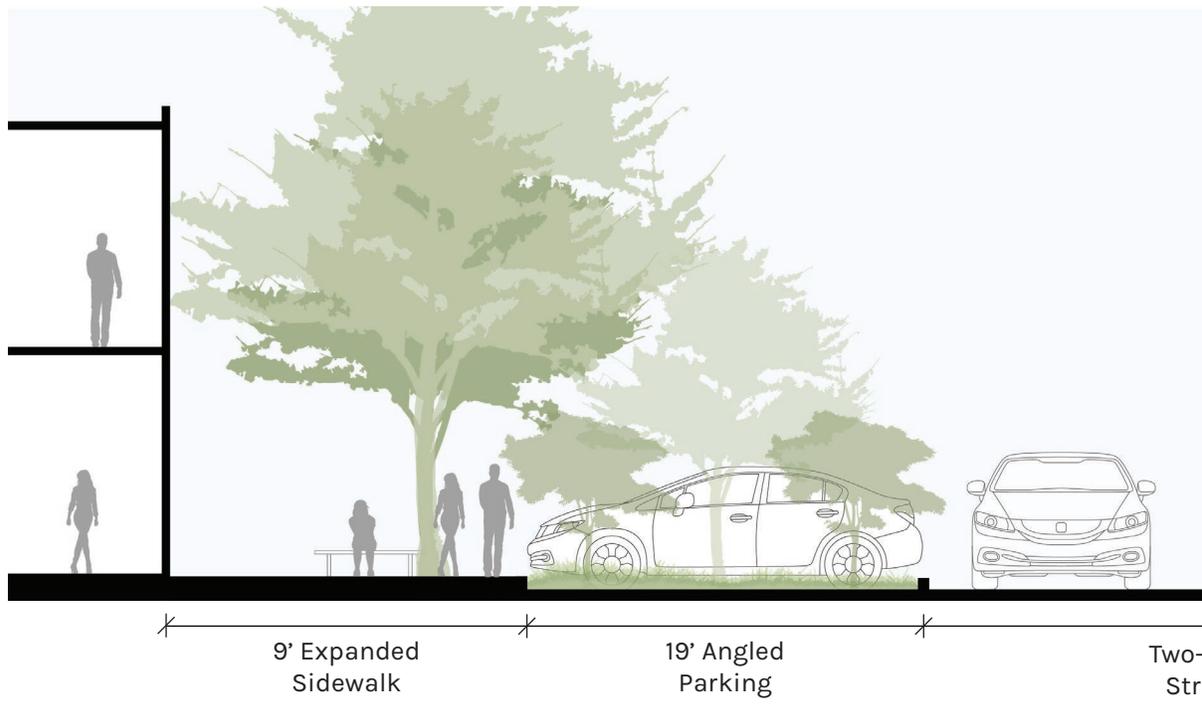


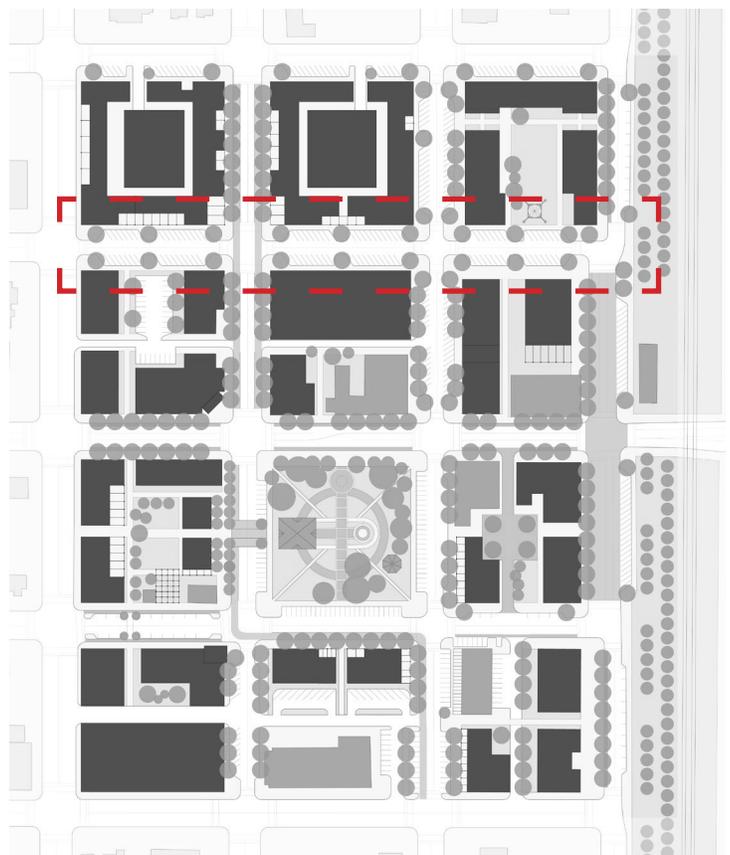
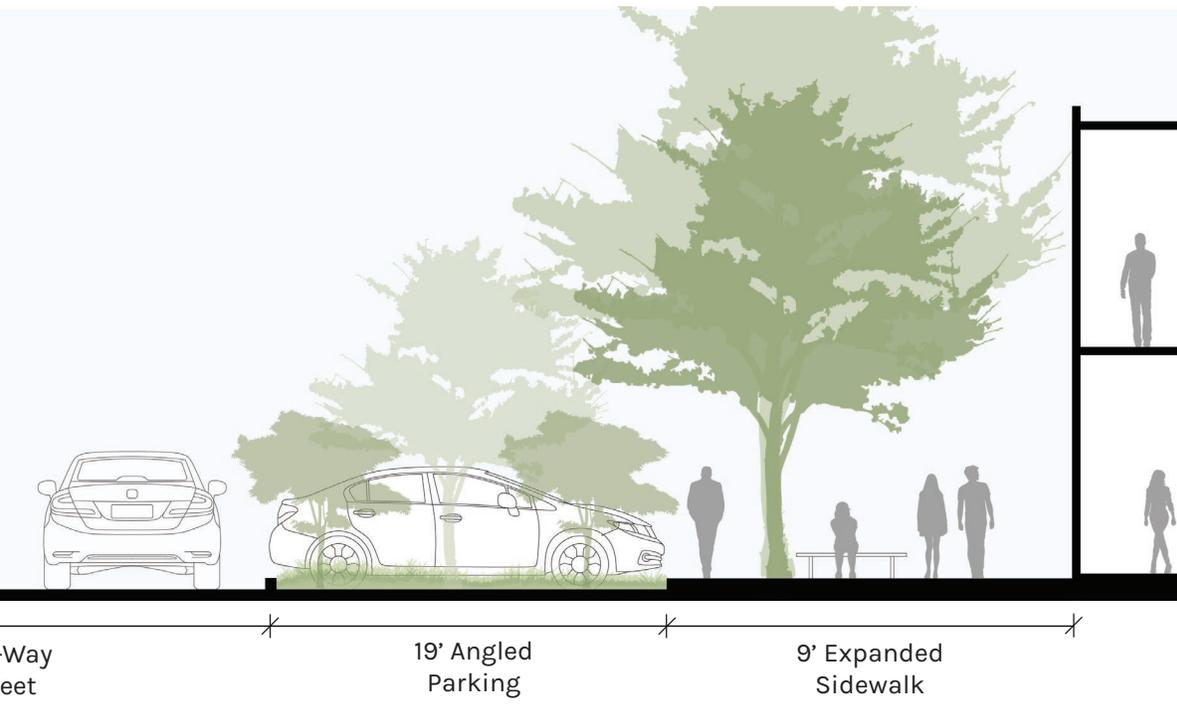
Burleson Street - Cross Section





Lockhart Street - Cross Section





1.16 STREET LIGHTING

Lighting is a key element in establishing the feeling of a place. The City of Kyle established a Lighting Ordinance (Ordinance No. 909) in 2016 that thoroughly details guidelines for new lighting installations, as well as how to address existing lighting that does not conform with the new standards. Additionally, the city has chosen and accepted a lighting fixture series, the DS770FC series, Dark Sky Acorn, by Amerlux* that will be the primary street lighting fixture used in the Downtown Master Plan.

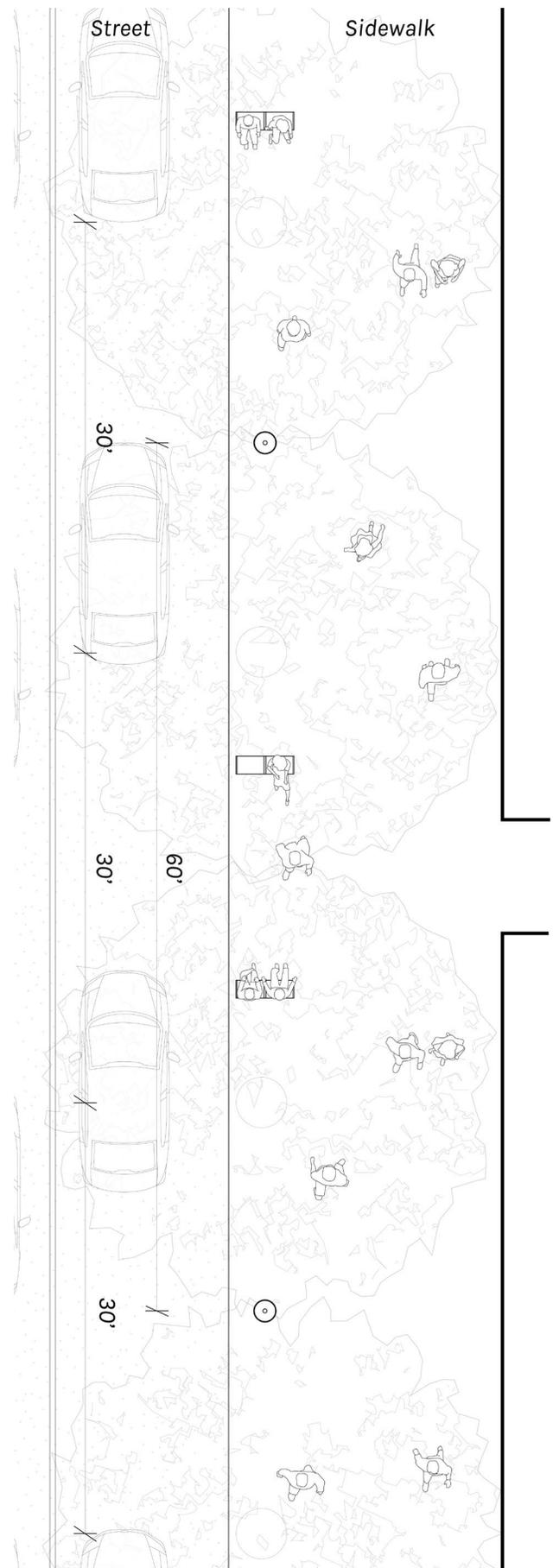
Street lamp spacing should be respectful of tree spacing. In the context of Downtown Kyle, this equates to street lamps being 60' on-center from one another, as shown in the diagram to the right.



Seasonal Tree Lighting



Full Cutoff Ground Fixtures



1.17 STREET FURNISHINGS

As downtown becomes increasingly more walkable and attractive to new retail and restaurants, the city will introduce new outdoor benches for the sidewalks.

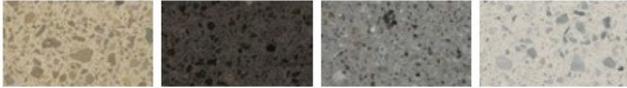
Center Street and Burleson Street within the extents of this master plan are recommended to have two benches per block in Phase 1 of the Downtown Master Plan.

Recommended bench:
“Abril” from Landscape Forms
A mixture of color combinations is recommended for variety:



Existing City of Kyle Bench

Cast Stone Acid-Etched Finish



Beige

Black

Grey

White



“Abril” from Landscape Forms shown in Beige

1.18 SERVICE AND UTILITY SCREENING

Switch Gear and Electric Transformers

Switch gear and transformers should generally be located on tertiary streets and service corridors/alleyways. If the equipment must be on a secondary street, they must be substantially setback from pedestrian corridors and screened with appropriate materials (artificial and/or landscaped with an accessible path to the device for maintenance), or decorated with commissioned public art. In general, switch gear, transformer pads, and other equipment should not be visible from or located on primary streets and sidewalks, especially those that are focused on pedestrian use.

Reduced Pressure Zone (RPZ) Devices

RPZ's shall be screened by encasing in an enclosure that is recommended by the manufacturer.

Grease Traps

Grease traps shall be carefully located and are not permitted along primary streets and their sidewalks, especially those that are focused on pedestrian use. They should be located within service zones or in setback areas along tertiary streets and/or alleys.

Trash and Recycling Receptacles

All trash and recyclables generated by a business shall be disposed of by that business in receptacles provided within the building or within the exterior space used by the building for seating, dining, advertisement, etc. Any exterior receptacles shall be made of durable, commercial grade materials and located against the building facade. These receptacles will not be permitted along the curb.



Lasercut Metal Gate for Utilities at Lancaster Mall, Weldcraft



Hedge screening Transformer Box from view



Hedge screening Transformer Box with adequate space between landscaping and box for maintenance

On-Street Trash Collection Spaces

On pick-up days dumpsters may be rolled into a special zone of the on-street parking area fronting a site for its trash/recycling collection, under the condition that this space is a dedicated, level area that will not allow the dumpsters to roll away from this designated location and that the dumpsters are returned to their usual location promptly after pick-up.

1.19 TRAFFIC & PARKING PROPOSALS

As the number of businesses and visitors Downtown potentially increases, its important that traffic and parking conditions can meet this increased demand. Additionally, 56% of respondents to the Downtown Master Plan Online Questionnaire stated that the traffic Downtown is “bad during special events or rush hour”, with another 27% stating that its traffic is “always bad”. These planned improvements will ensure that Downtown has the infrastructural capacity to meet this growth.

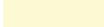
Traffic and Parking Goals

- Align pedestrian thoroughfares, sidewalk extensions, and bike lanes with The Vybe, Kyle’s planned trail network
- Increase the amount of street parking within the Downtown Core while maintaining Center St. and Burleson St. as walkable, pedestrian-centered thoroughfares.
- Consolidate parking into modern parking garages that are within a 5-minute walk of Center Street.
- Improve traffic circulation and hierarchy of Downtown streets by refining streetscape conditions.

Public Parking Tabulations

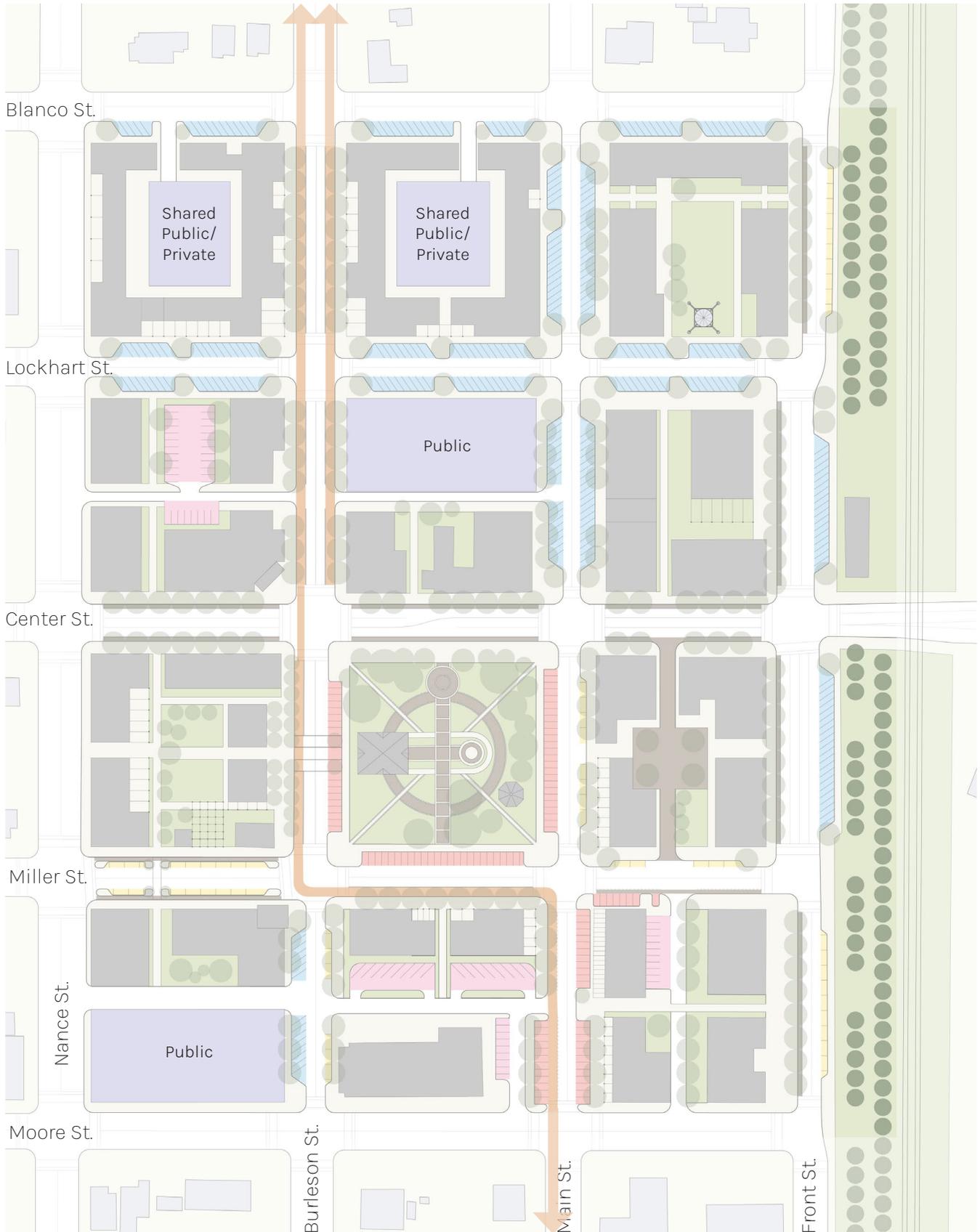
Street Name	Street Parking	Parking Garage
Center St	0	-
Front St	55	-
Main St	137	-
Burleson St	34	-
Lockhart St	91	300
Blanco St	43	-
Miller St	53	-
Moore St.	-	300
Public / retail parking in multifamily garages	-	120
	413	720

GRAND TOTAL: 1,133 spaces
PREVIOUS TOTAL: 376 spaces
ADDED PARKING: 757 spaces

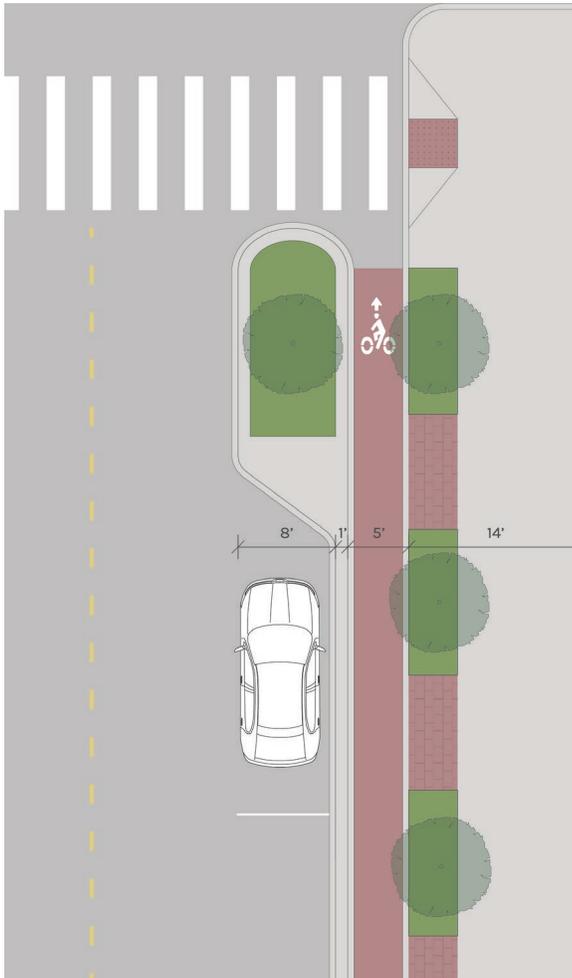
	Parallel Parking
	Angled Parking
	Head-In Parking
	Parking Lot
	Parking Garage
	Vybe Trail
	Bike Lanes

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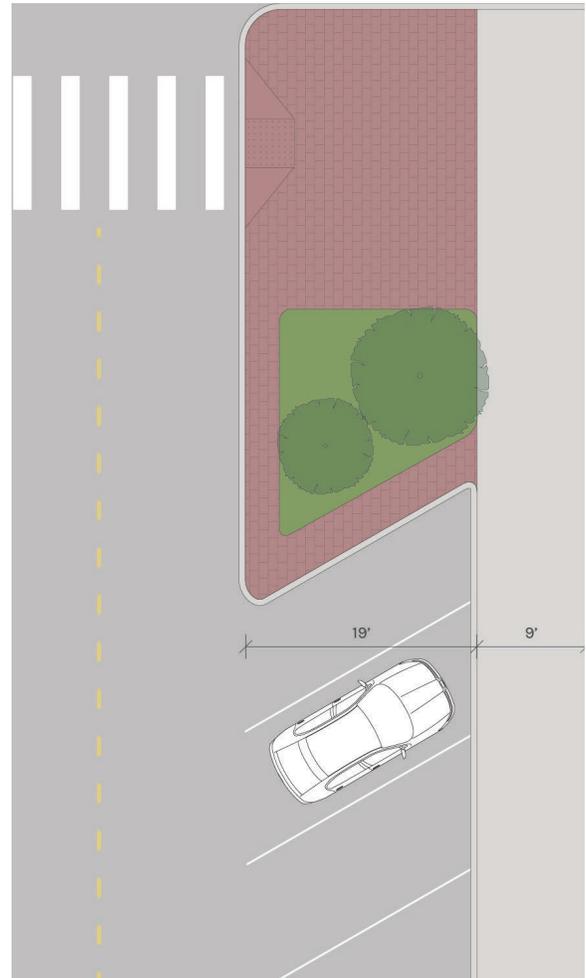
Parking Location Map



Parking Conditions



Standard Parallel Parking Condition



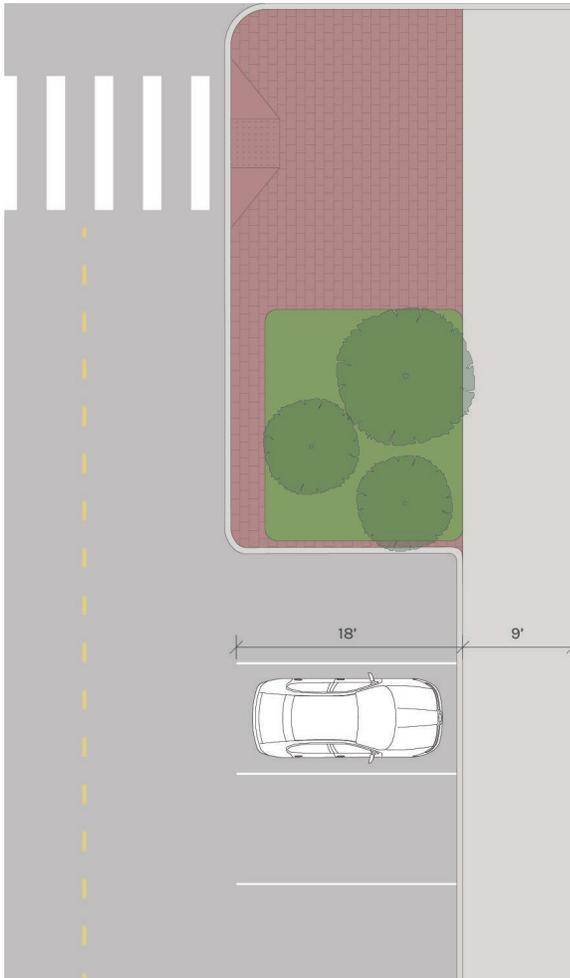
Angled Parking Condition

Parallel Parking

To promote pedestrian and cyclist movement across Downtown, the majority of streets that have parallel parking will feature a protected bike lane to delineate space between the road and pedestrian corridors. These bike lanes will be protected through the use of street parking and/or raised medians, and will be easily visible through the use of terracotta paving and/or paint along the lanes.

Angled Parking Condition

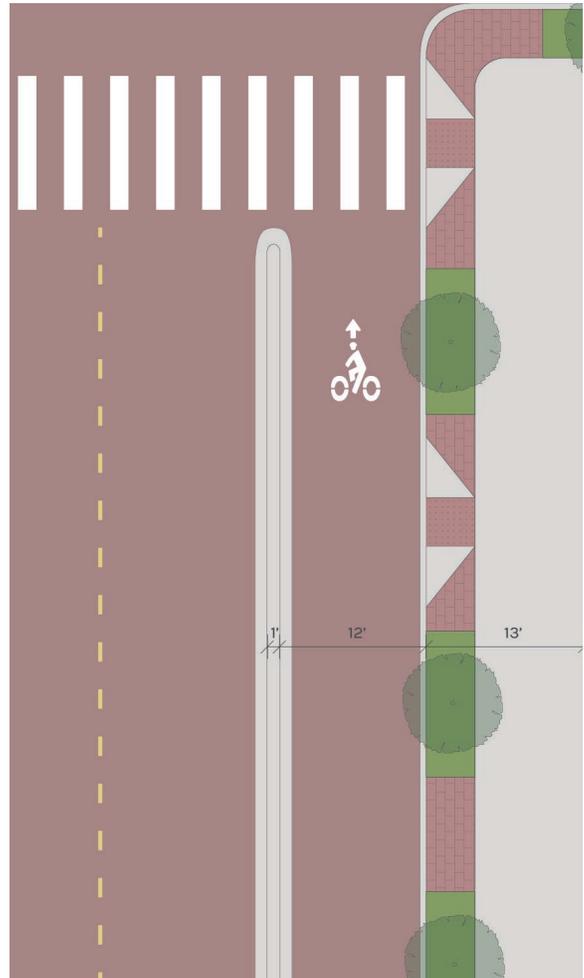
The angled parking condition will generally be present along streets with less foot traffic and that will have businesses and residents that would be best served by having parking so close-by. This type of parking is preferred over 90-degree head-in parking due to the ease of parking and exiting from stalls, as well as the lowered incidence of accidents observed with this parking type.



Head-In Parking Condition

90-Degree Head-In Parking Condition

The 90-degree head-in parking condition will be present along the perimeter of Mary Kyle Hartson Park and at some developments that have been built within the last several years. Generally, all 90-degree spaces will be previously built; to reduce the amount of construction and imposition placed on residents by it, these spaces that were built so recently will remain and continue to serve the buildings they were built for.



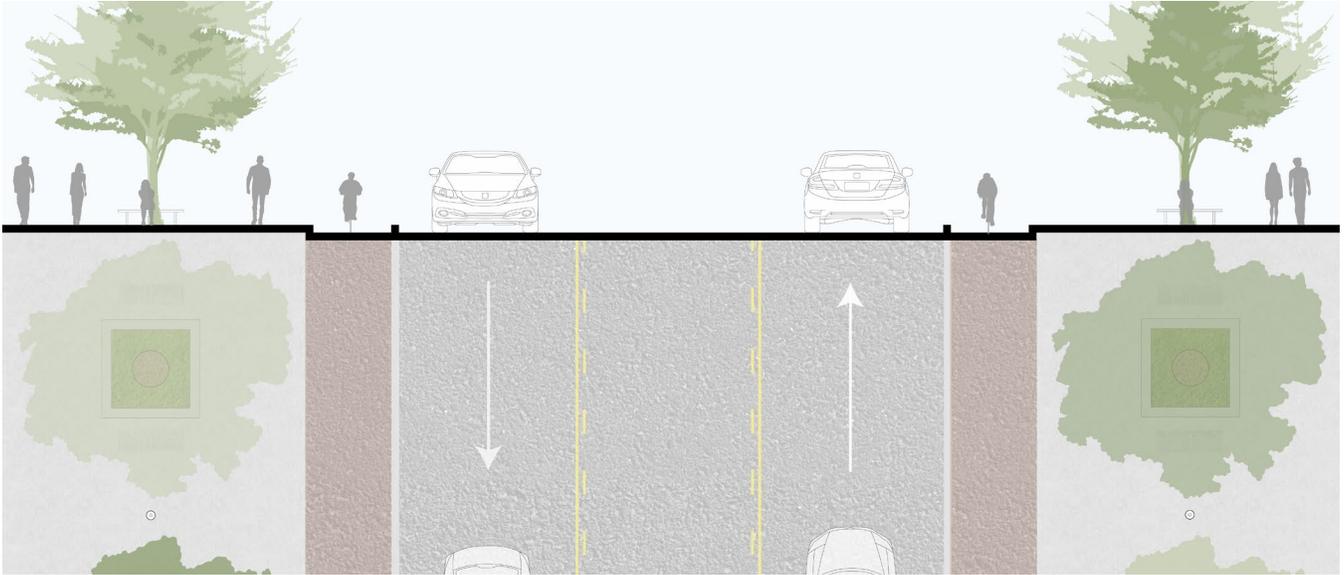
Burleson Street Condition

Burleson Street Condition

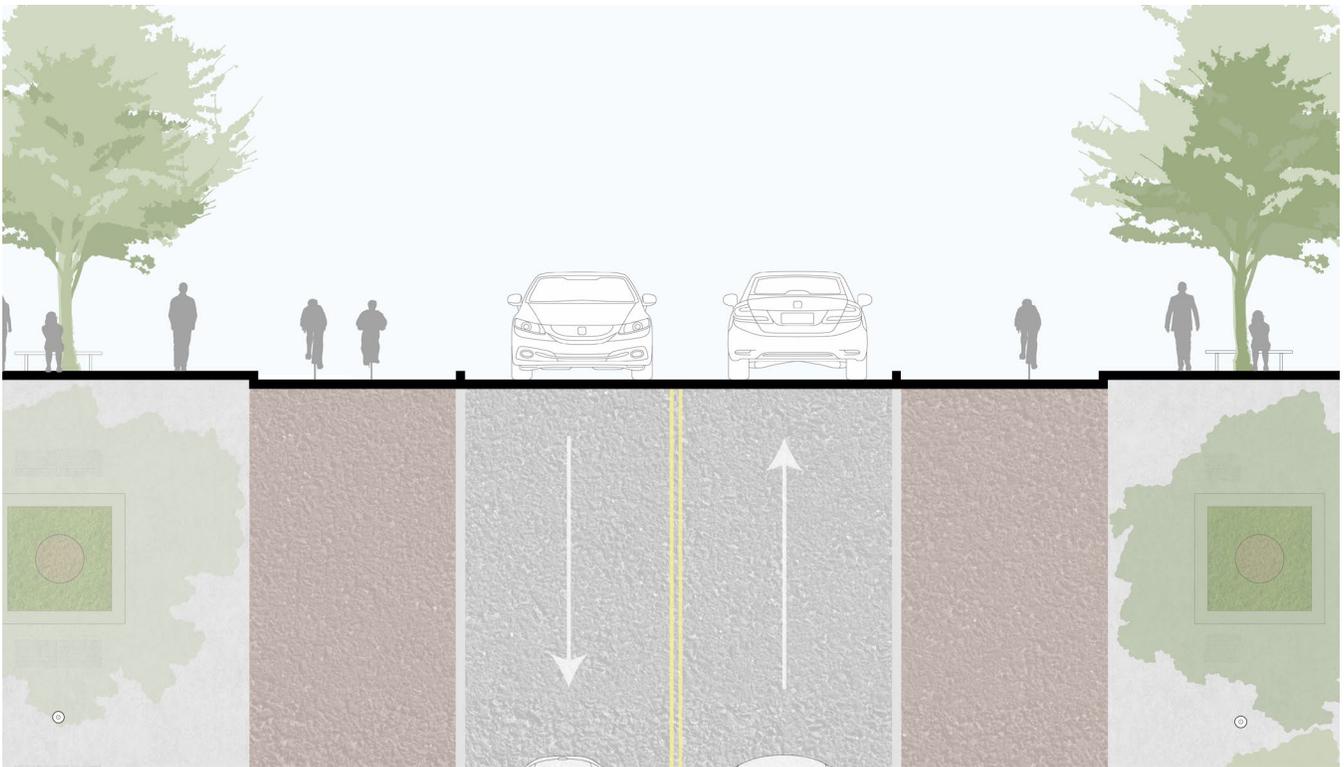
Burleson Street will have a special condition for its layout - since this street is along The Vybe and will serve as Downtown's main connection to it, the design is pedestrian- and cyclist-focused with no parking along its length. The bike lane also acts as a paved trail condition and is 12 feet wide to accommodate golf carts and small vehicles that are allowed on the trail.

Flow of Traffic Diagrams

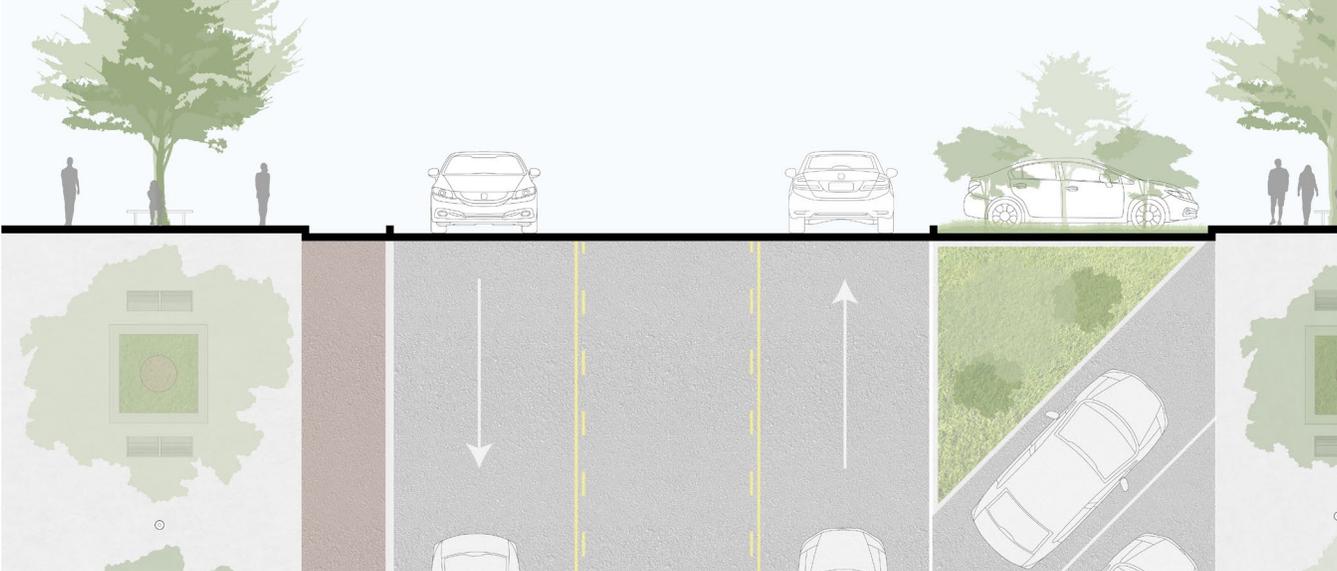
Center Street



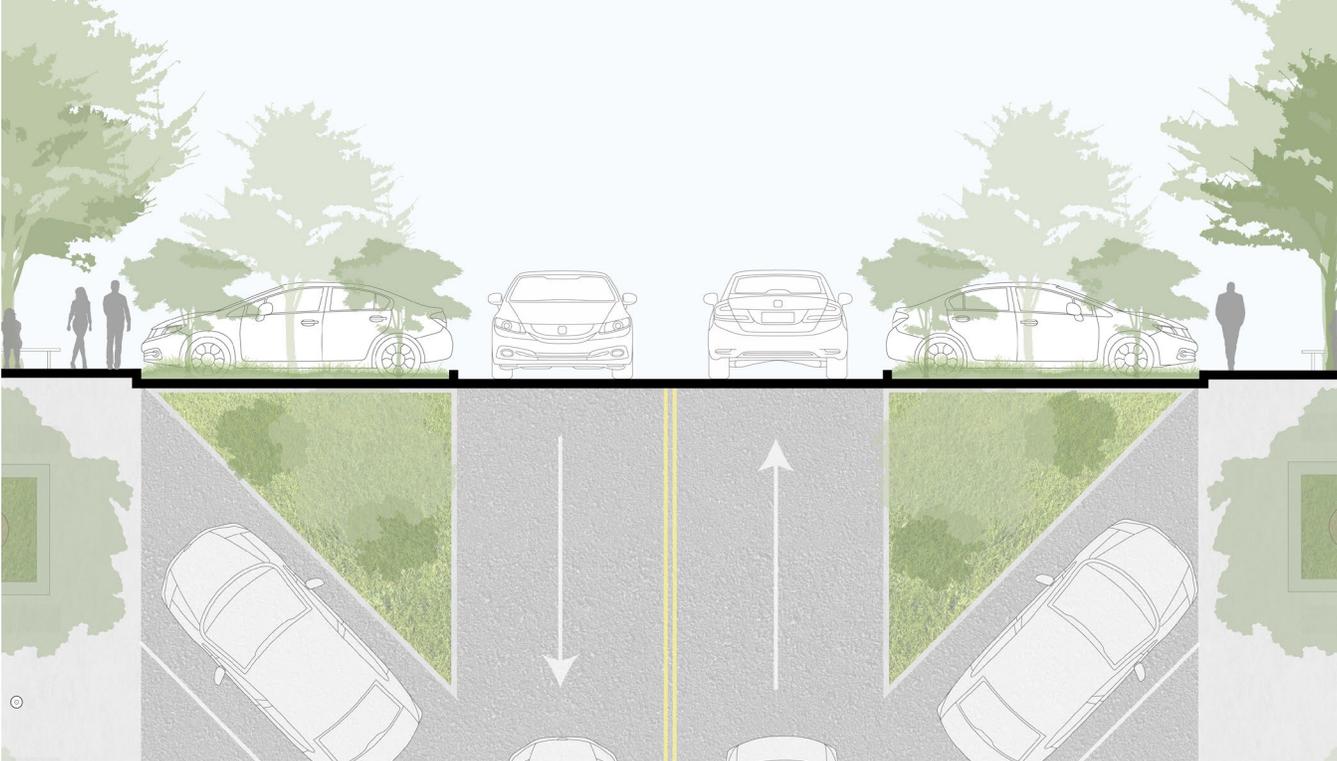
Burleson Street



Front Street



W Lockhart Street



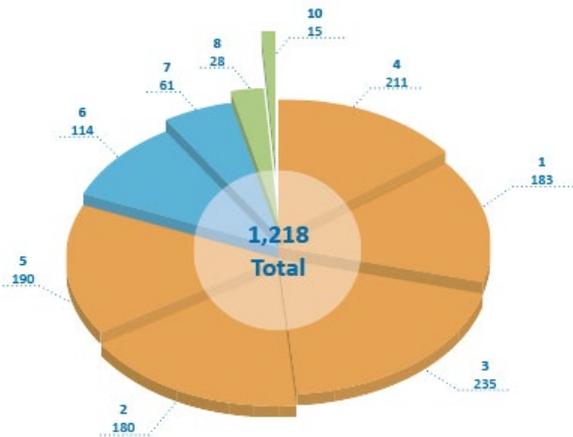


**DOWNTOWN
MASTER PLAN
APPENDIX**

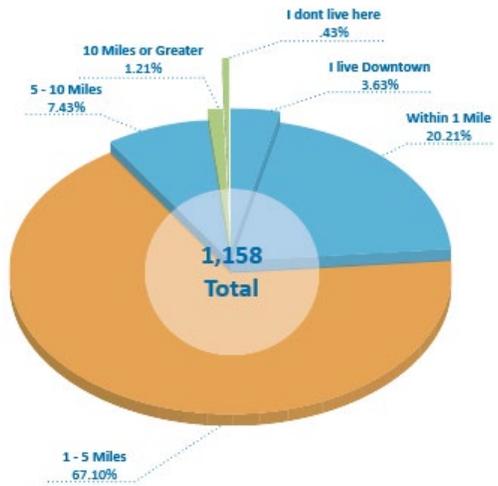
1.20 PUBLIC QUESTIONNAIRE (SURVEY)

1. On a scale of 1 to 10, how vibrant is your downtown?
 2. How far from downtown Kyle do you live?
 3. How would you describe the traffic downtown?
 4. What is your favorite thing to do downtown? (Open-ended question)
 5. How often do you visit downtown?
 6. What time of day do you typically visit downtown?
 7. What is the biggest obstacle to visiting downtown more often?
 8. What would you like to see in downtown Kyle? (You may choose more than one.)
- Please rate the statements in Questions 9-15 with a number from 1-10 (1 = strongly disagree, 10 = strongly agree)*
9. Downtown is a great place to be
 10. I'm proud to call Kyle home
 11. Downtown Kyle has everything I need
 12. Downtown Kyle is family-friendly
 13. Downtown is perfect as it is today
 14. I would like downtown to be bigger
 15. I think downtown should have wider sidewalks with seating and street trees
 16. Please rank the values that are most important to you, with 6 being the most important, and 1 being least important.
 - Fostering a Sense of Community
 - Improved Safety
 - Improved Traffic Conditions
 - Local Business Ownership
 - Practicing Environmental Sustainability
 - Walkability & Visually Appealing Streetscapes (Walkability refers to the ability to safely walk to amenities/shops/schools within a reasonable distance)
 17. What is your age?
 18. How many people are in your household?
 19. How long have you lived in Kyle?
 20. What are your favorite memories of downtown Kyle? (Open-ended question)

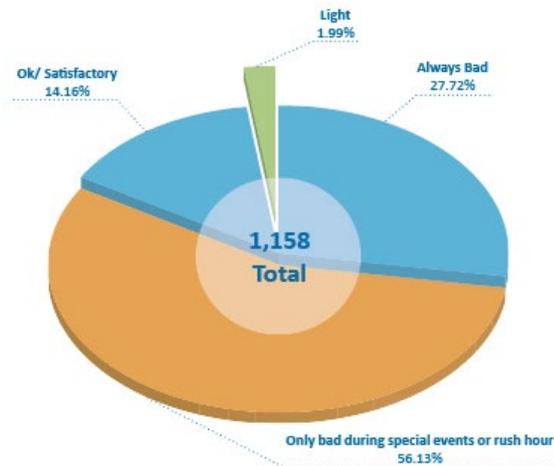
1) On a scale of 1 to 10, How vibrant is your Downtown?



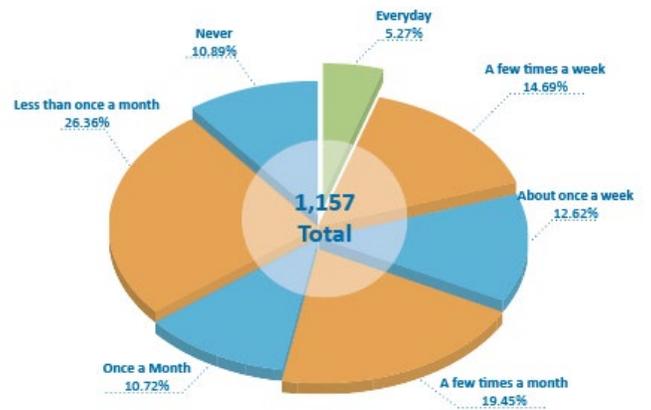
2) How far from downtown Kyle do you live?



3) How would you describe the traffic downtown?



5) How often do you visit Downtown Kyle?

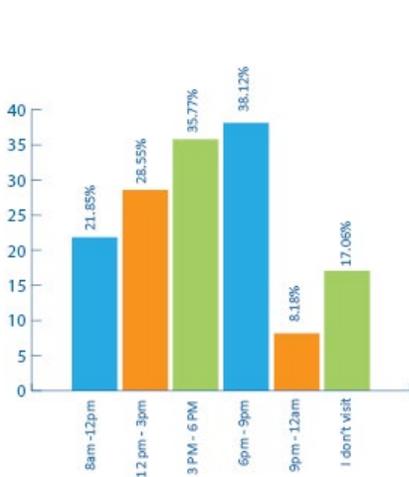


4) What are your favorite things to do in downtown Kyle?

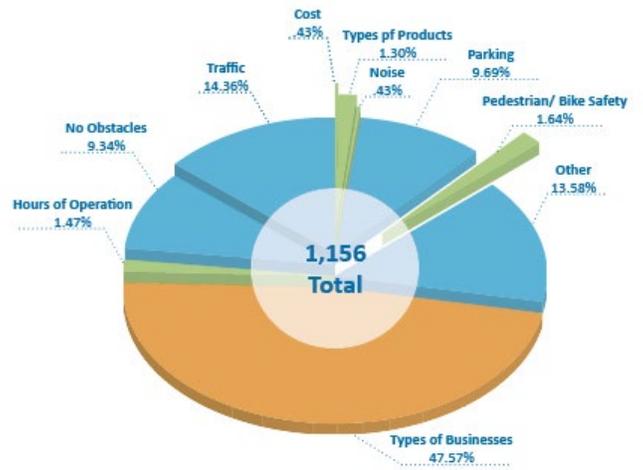
museum Trader Joe s dining dog park Community restaurants bakeries need nice Please
 Winery Something go Book store Cafe boutiques Bookstore food trucks
 retail family friendly non-chain s things stores grocery store Buda
 indoor family Kyle bars activities restaurants unique
 shop local downtown good park Farmer markets food
 sit restaurants place Stop space new businesses love
 entertainment area Ice cream variety coffee shop Non Children venues
 kids theater Nothing gift shops APARTMENTS friendly spaces events square

APPENDIX

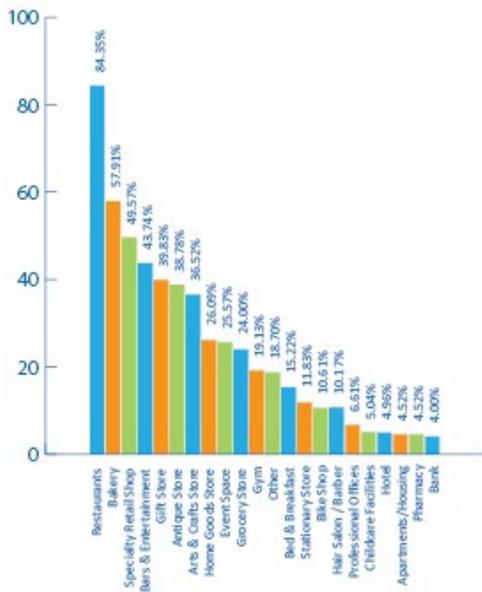
6) What time of day do you typically visit downtown?



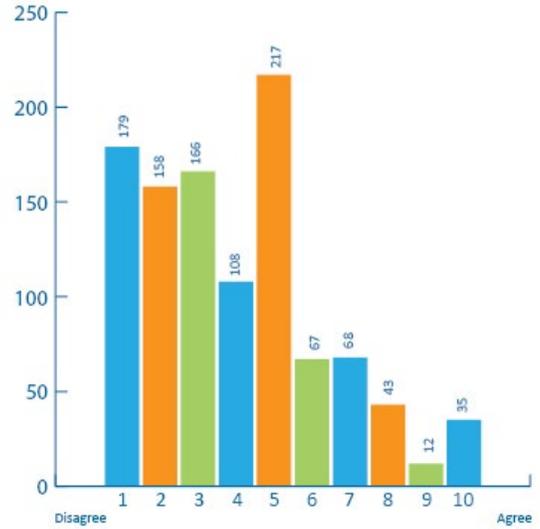
7) What is the Biggest Obstacle when visiting Downtown?



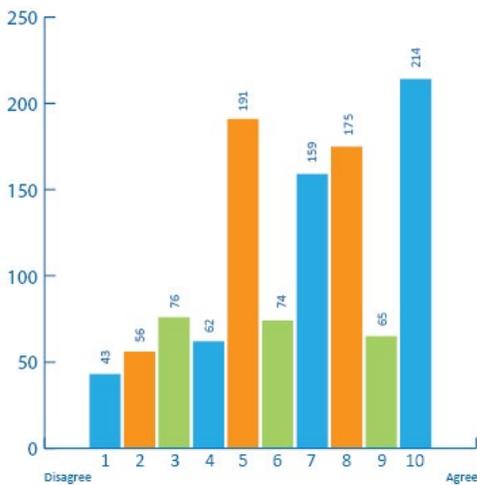
8) What would you like to see in downtown Kyle?



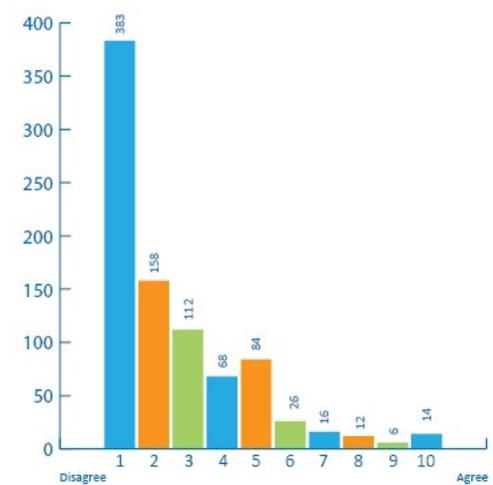
Downtown is a great place.



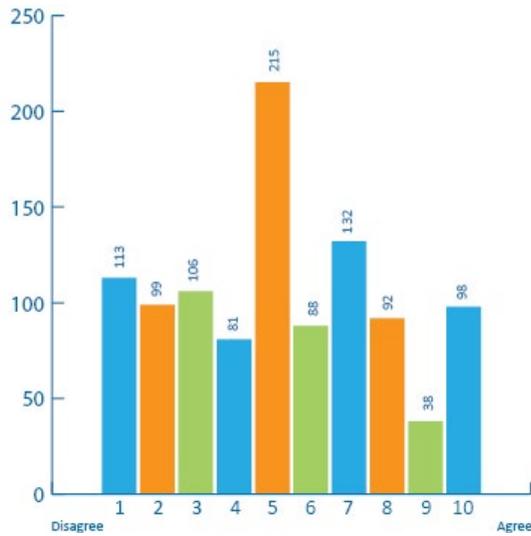
10) I'm proud to call Kyle home.



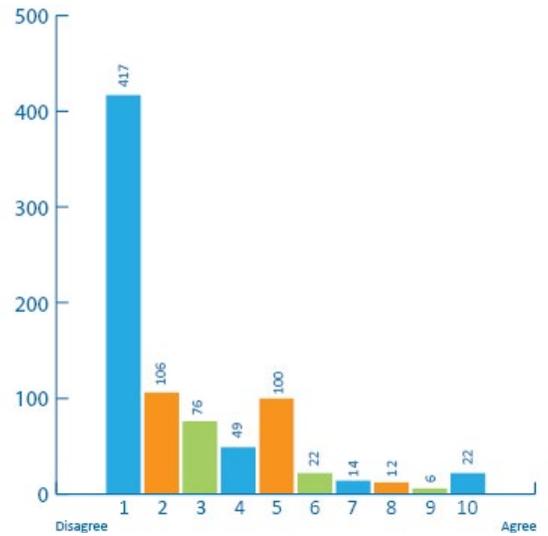
11) Downtown has everything I need.



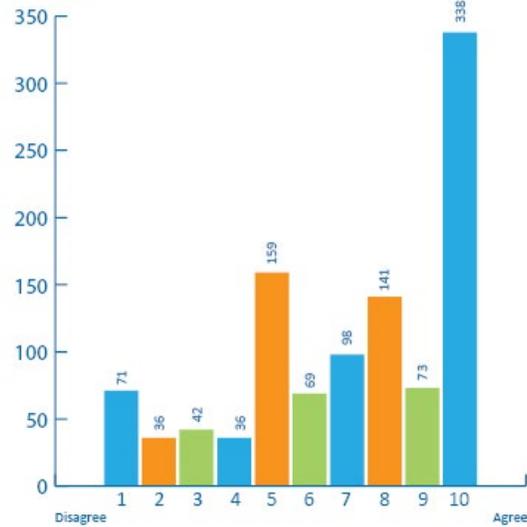
12) Downtown is family friendly.



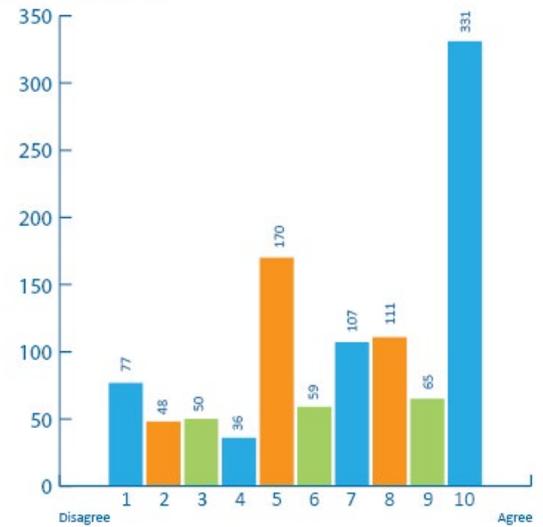
13) Downtown is perfect.



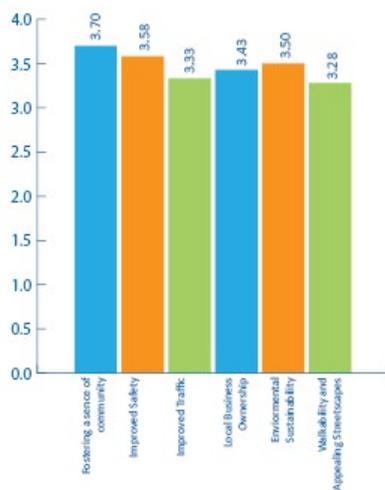
14) I would like downtown would to be bigger.



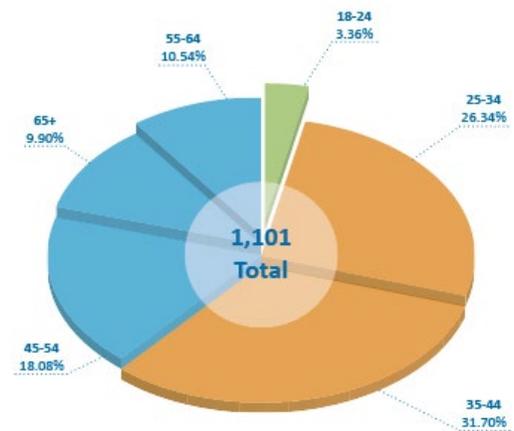
15) I think downtown should have bigger sidewalks.



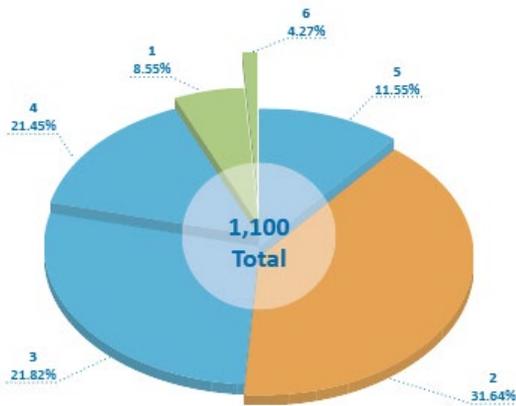
16) Please rank the values that are most important to you, with 6 being most important, and 1 being least important.



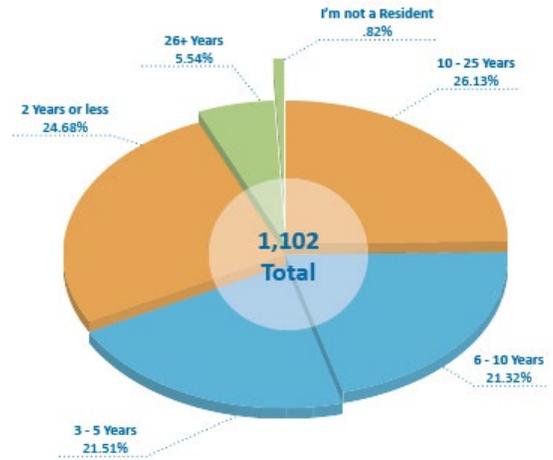
17) What is your age?



18) How many people are in your household?



19) How long have you lived in Kyle?



20) What are your favorite memories of downtown Kyle?





Flag Day on Center Street